



### 3. REGULATORY SERVICES

#### 3.1 PLANNING

##### 3.1.1 Residential Design Guidelines

<b>Type:</b>	Regulatory Services - Planning
<b>Legislation:</b>	Town Planning & Development Act 2005 Planning & Development (Local Planning Schemes) Regs 2015
<b>Delegation:</b>	
<b>Other Related Document:</b>	Local Planning Scheme No 3



REFER TO SEPARATE POLICY DOCUMENT

<b>Responsible Directorate:</b>	Regulatory Services
<b>Reviewing Officer:</b>	Executive Manager Regulatory Services
<b>Decision making Authority:</b>	Council
<b>Policy Adopted:</b>	18/09/12
<b>Policy Amended/Reviewed:</b>	18/03/13, 17/03/15, 17/05/16
<b>Former Policy No:</b>	2.2.2

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## 2.0 Introduction

The following document was the product of a report on Residential Development Guidelines December 2011. The Residential Development Guidelines were part two of a two stage process. The Residential Development Guidelines describes the background to, and the development of, new Residential Design Guidelines for the Town of East Fremantle (the Town).

## 3.0 Local Planning Policy (Design Guidelines for Residential Development)

### 3.1 Introduction

Residential development within the Town has previously been controlled by the provisions of the Town of East Fremantle Town Planning Scheme No. 3, the Residential Design Codes and more than twenty Local Planning Policies.

This Local Planning Policy has been prepared by the Town to build on the development requirements contained in those documents, and consolidates them into a single, comprehensive and updated Policy.

The new Policy has taken into consideration community concerns regarding the incompatibility of some new dwellings and additions to existing dwellings. Community forums have raised the issue that some recent developments have disrupted or adversely impacted on traditional streetscapes and neighbourhood character from time to time.

Relevant principles contained in State Planning Policy 3.5 'Historic Heritage Conservation' have also been included in this Policy where appropriate.

### 3.2 Purpose of this Policy

The purpose of this Local Planning Policy is:

- i. to ensure orderly development within the Town,
- ii. to ensure that the preparation and assessment of applications for planning approval for residential development in the Policy Area (refer glossary) achieve the objectives of the Policy in an accountable, comparable and consistent manner;
- iii. to provide guidance to landowners and the Town in preparing and assessing applications for planning approval within the Policy Area;
- iv. to ensure that places of heritage (refer glossary) value that contribute to the domestic scale and architectural character of the Policy Area are retained and that, where adaptations or extensions are necessary to ensure their ongoing viability, development does not reduce the heritage value of the places or their contribution to the streetscape and character of the Policy Area; and,
- v. to provide added protection for the established residential character of each of the Precincts that make up the Policy Area.

This Local Planning Policy builds on the development requirements (Acceptable Development and Performance Criteria) of State Planning Policy 3.1 'Residential Design Codes', in order to ensure consistency between State and Local Planning Policy approaches in conserving the character and amenity of the Policy Area. Relevant provisions of State Planning Policy 3.5 'Historic Heritage Conservation' have also been included in this Policy where appropriate.

### 3.3 Objectives of this Policy

- i. To conserve and protect individual residences considered to have significant heritage value;
- ii. To provide development and design guidance to landowners wanting to extend or refurbish existing dwellings of heritage significance;
- iii. To guide additions to existing dwellings, which do not adversely affect the significance of the dwelling, or of neighbouring heritage places;
- iv. To guide new dwellings and additions/alterations to existing dwellings (particularly second storey additions), which are compatible with the character, form and scale of existing residential development in the locality, and harmonise with the existing streetscape; and,
- v. To encourage creative design solutions of quality that meet the standards of this Policy, and which enhance the character of the residential precincts and Policy Area.

### 3.4 Local Planning Policy Area



**Figure 1** – East Fremantle Satellite Photograph- *Nearmap - November 2011*

The Local Planning Policy Area is bound by Jerratt Drive, Petra Street, Marmion Street, East Street and Riverside Road.

### 3.5 Precincts

The Local Planning Policy and its guidelines apply to the following precincts:

- i. Plympton;
- ii. Woodside;
- iii. Richmond;
- iv. Richmond Hill;
- v. Riverside;
- vi. Preston Point; and,
- vii. Richmond Raceway.

Refer Figure 2 for precinct boundaries.

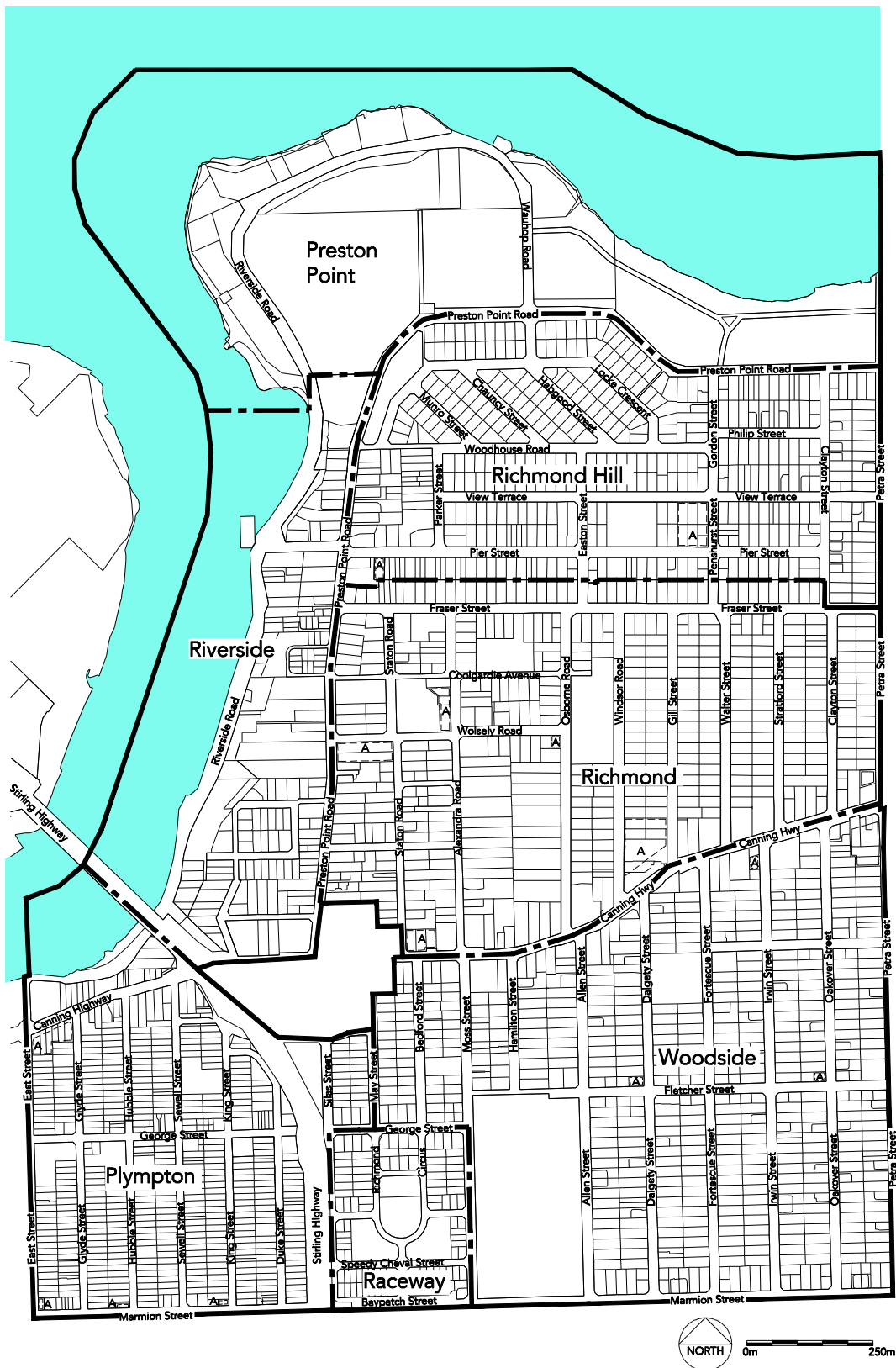


Figure 2 – East Fremantle Precinct Location Plan – Griffiths Architects - November 2011

Identifies the major residential precincts; Plympton, Woodside, Richmond, Richmond Hill, Riverside, Preston Point and Richmond Raceway and their locations within the Town of East Fremantle boundaries.

### **3.6 Status/Application of this Policy**

This Local Planning Policy has been adopted under Clause 2.4 of Town Planning Scheme No. 3. From this point on, Town Planning Scheme No.3, shall be referred to as “the Scheme”. This means that both the Town and prospective applicants are obliged to take the Policy into account. Variations and departures from the Policy will need to be suitably justified by both the applicant and the Town.

With the exception of those aspects of development to which the Residential Design Codes apply (in respect to the Policies desired outcomes and performance criteria), the provisions of this Local Planning Policy apply to all development and/or use of land in the Policy Area (refer glossary) excluding the Town Centre Precinct where indicated as permitted or discretionary under the Zoning Table in the Scheme.

Subject to Part 5 ‘Local Planning Policies of the Residential Design Codes’, the provisions of this Local Planning Policy augment the Codes by providing additional Performance Criteria and Acceptable Development provisions for aspects related to heritage, streetscapes, building design/appearance, boundary walls, site works, building heights and external fixtures.

Subject to the Scheme, development and use of land within the Policy Area excluding the Town Centre Precinct shall be in accordance with the standards and requirements of this Local Planning Policy, and except as otherwise provided, shall require the prior planning approval of the Town in accordance with the provisions of the Scheme.

In considering an application for planning approval where a variation is proposed to one or more Acceptable Development requirements prescribed under the Residential Design Codes and/or this Local Planning Policy, the Town shall, unless it otherwise determines, as part of any application for planning approval require the lodgement of a Development Impact Statement which has been prepared at the applicant’s expense by a suitably qualified and experienced consultant, with the Statement being to the Council’s satisfaction.

This Local Planning Policy applies to all places within the Policy Area excluding the Town Centre Precinct, including those places entered in the State Register of Heritage Places and Commonwealth Heritage List.

### **3.7 General Standards for Land Use & Development/Policy Statement**

#### **3.7.1 Residential Development in the Town of East Fremantle Policy Area**

The development of residential land is to be in accordance with the requirements of the following:

- i. This Local Planning Policy;
- ii. The R-Codes for residential development, subject to the variations;
- iii. The applicable zone provisions under the Scheme;
- iv. Relevant Local Planning Policies or Local Laws;
- v. State Planning Policy 3.5 – Historic Heritage Character;
- vi. Applicable Australian Standards; and,
- vii. Burra Charter principles in the case of heritage premises.

The following provisions apply to development within all Precincts of the Policy Area except for the Preston Point Precinct and the Town Centre Precinct. In areas subject to the LPP Town Centre Redevelopment Guidelines, those Guidelines take precedence and are additional to the requirements of this document.



### 3.7.2 Additions and Alterations to Existing Buildings

#### 3.7.2.1 Statement

*The Town supports well designed alterations and additions to existing buildings within the Policy Area. Lean-to additions are generally acceptable.*

*Second storey additions are acceptable within the Policy Area. For traditional contributory buildings additions must either be accommodated within the existing roof space or not be dominant from the primary street.*



#### 3.7.2.2 Desired Development Outcomes

- i. Additions and alteration should take into account the significance and character of the existing building and its contribution to the character of the Precinct;
- ii. Additions and alterations should be well designed with minimal interference to the existing building;
- iii. Single storey additions and second storey additions and alterations to existing dwellings are acceptable. Second storey additions shall be supported but are required to:
  - a. Be constructed within the existing roof space, or towards the rear of the dwelling and must not impact upon significant fabric of the dwelling; and,
  - b. Not be dominant from the primary street.
- iv. Additions and alterations should visually contrast to a contributory dwelling. Differentiation may be major or subtle; and,
- v. Additions and alterations should always respect scale, bulk and proportions of the existing dwelling.

**NOTE:** All applications for planning approval for additions and alterations are to comply with all other design elements of this LPP.

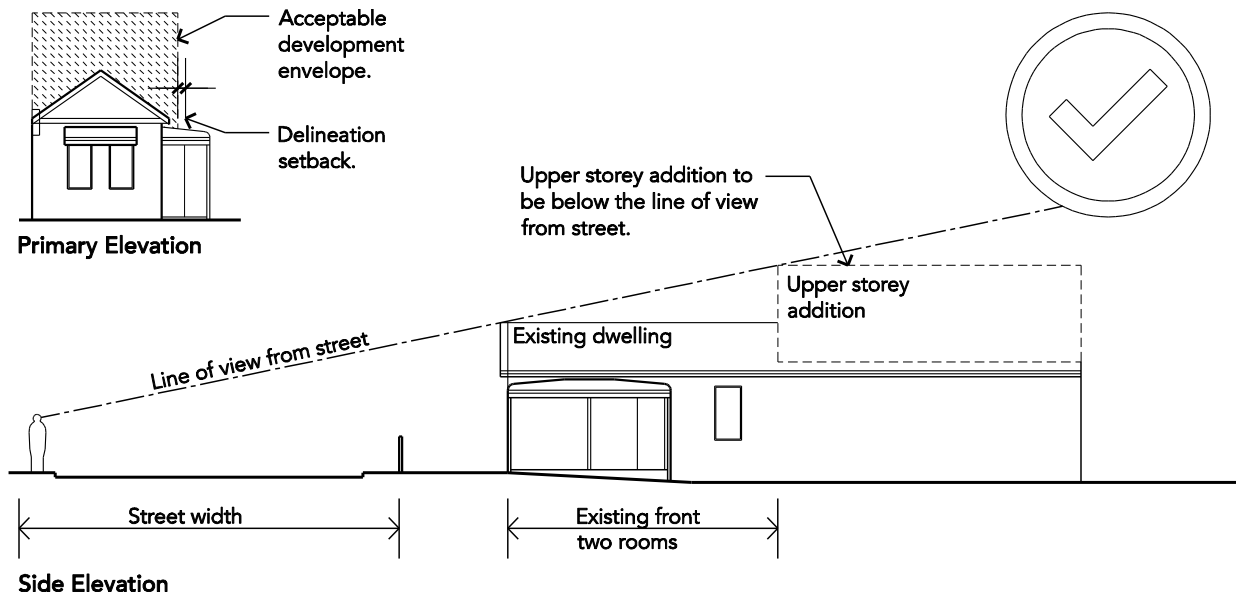


Figure 3 - Elevations illustrating compatible alterations and additions - *Griffiths Architects 2012*. The actual configuration of such an extension is subject also to R Code setback provisions.

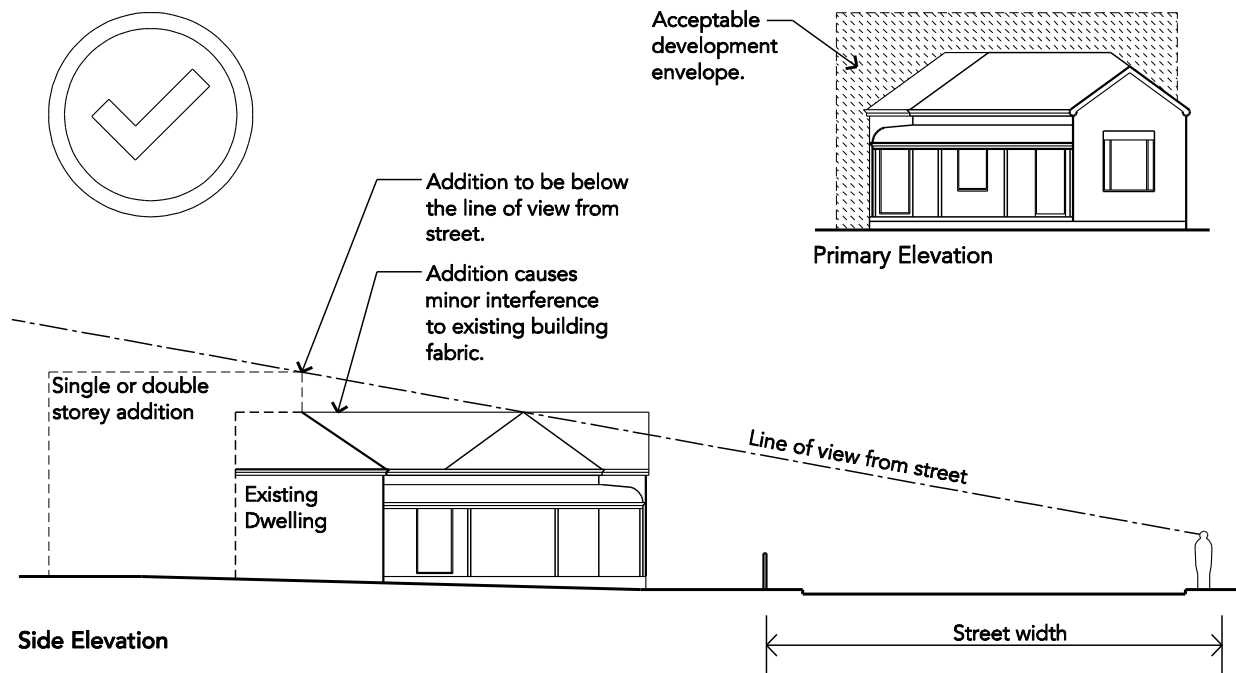


Figure 4 - Elevations illustrating compatible alterations and additions - *Griffiths Architects 2012*.

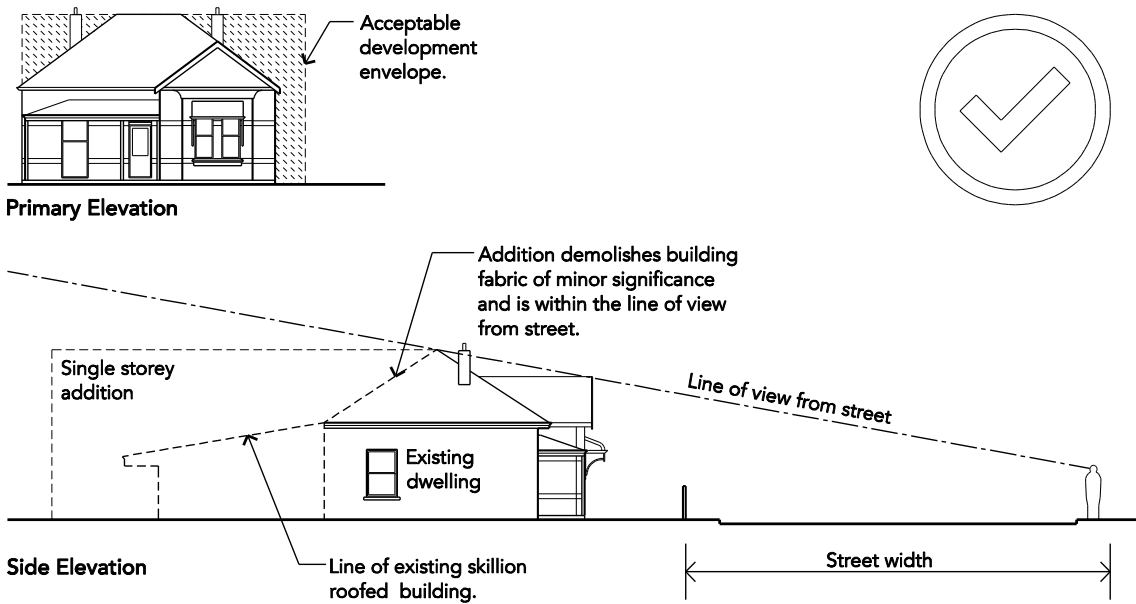


Figure 5 - Elevations illustrating compatible alterations and additions - Griffiths Architects 2012.

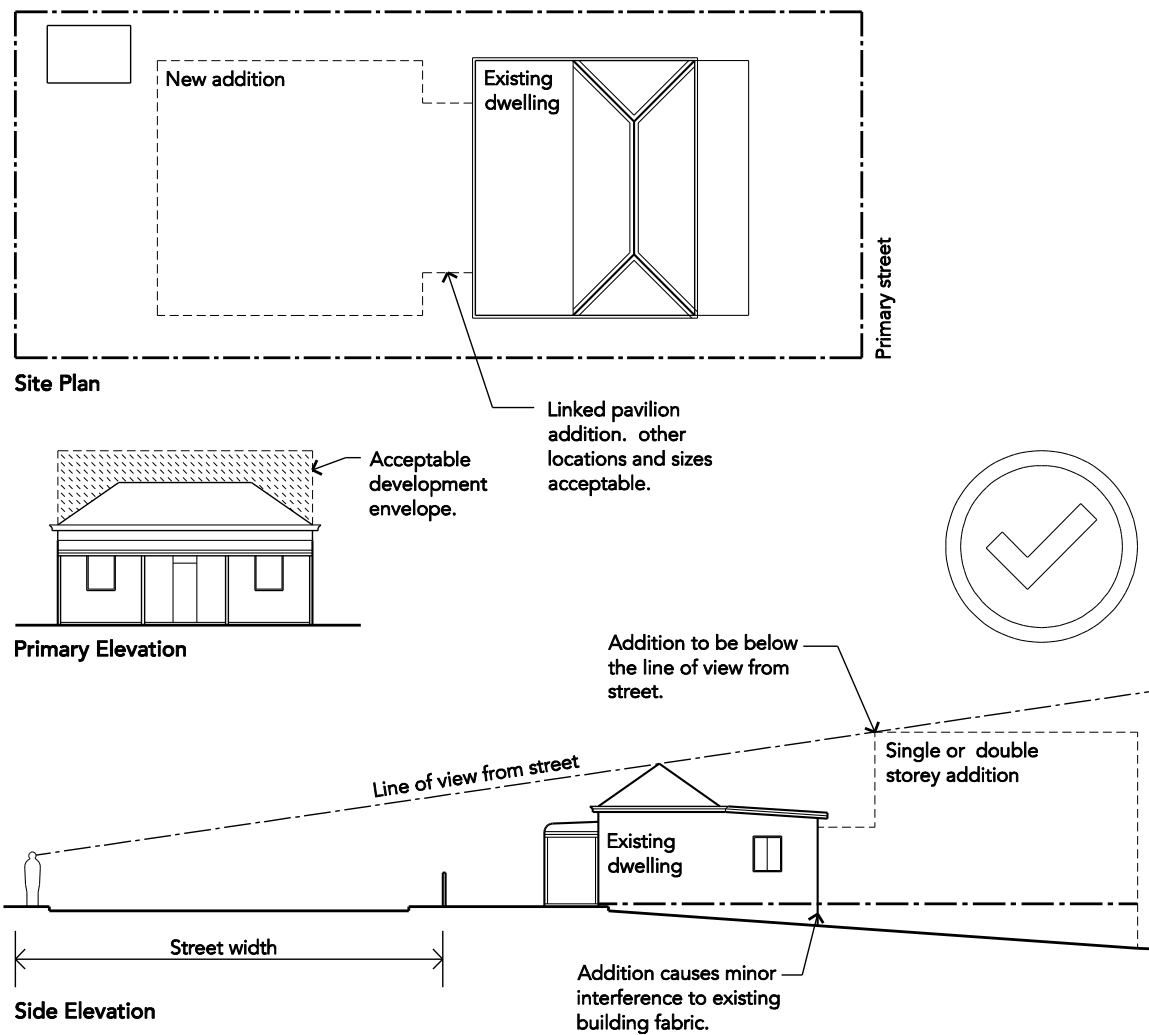


Figure 6 - Plan and elevations illustrating compatible alterations and additions - Griffiths Architects 2012.

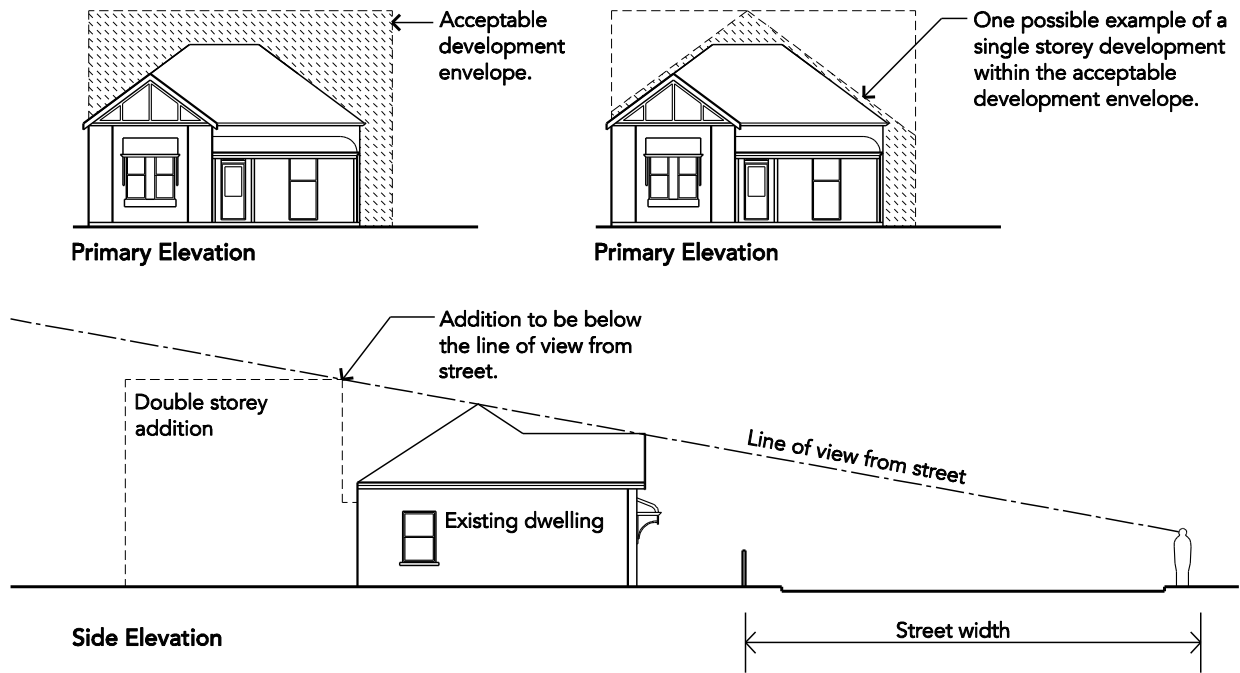


Figure 7 - Elevations illustrating compatible alterations and additions - *Griffiths Architects 2012*.

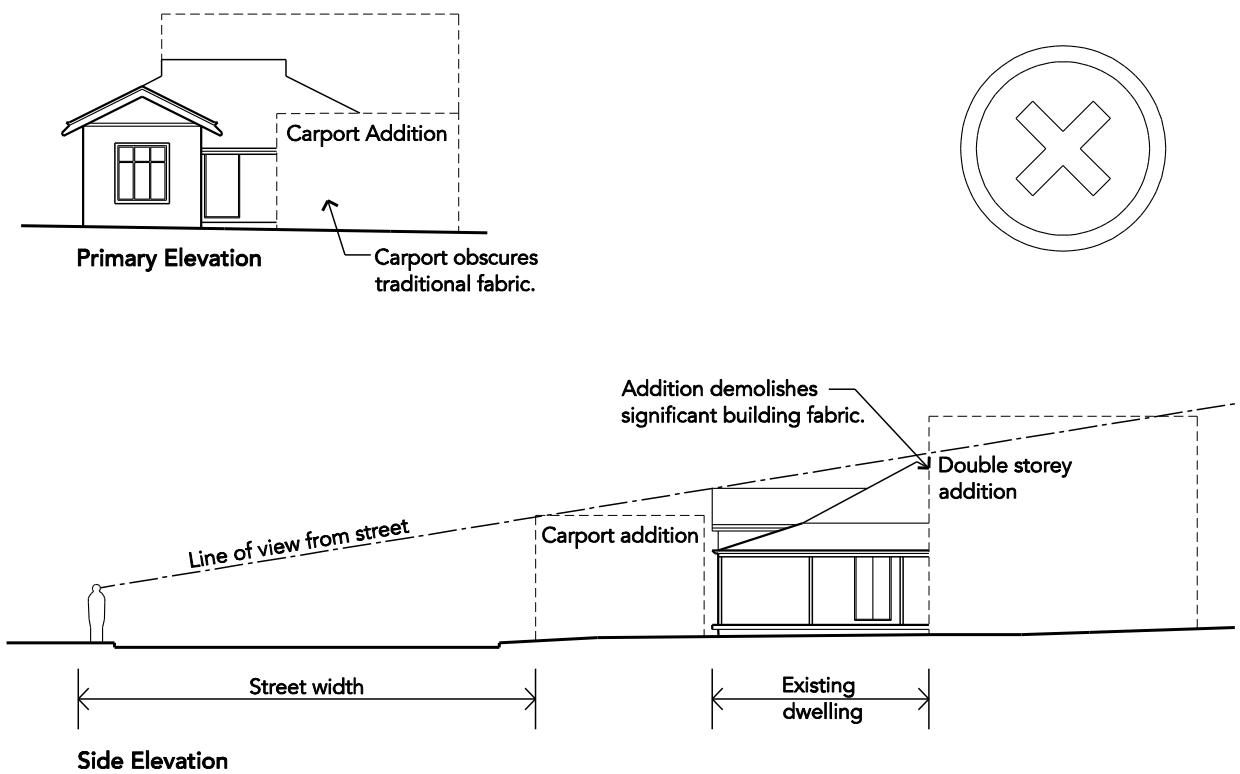


Figure 8 - Elevations illustrating incompatible alterations and additions - *Griffiths Architects 2012*.

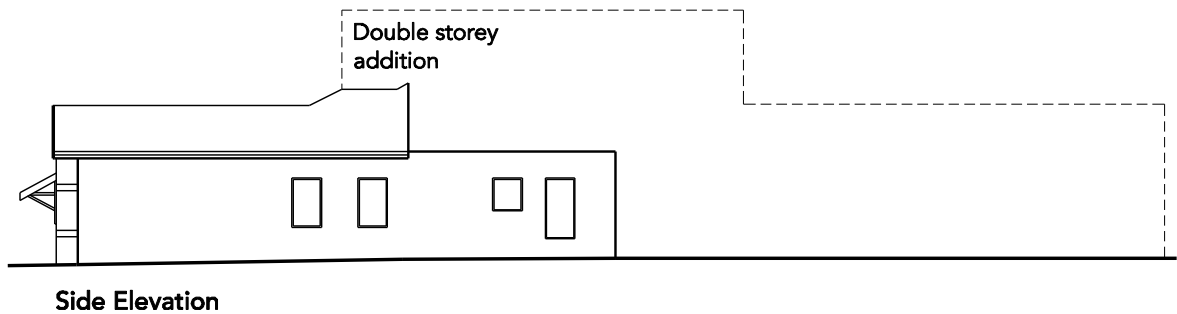
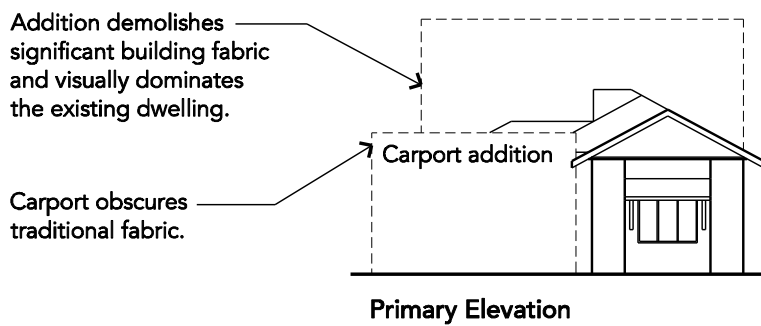
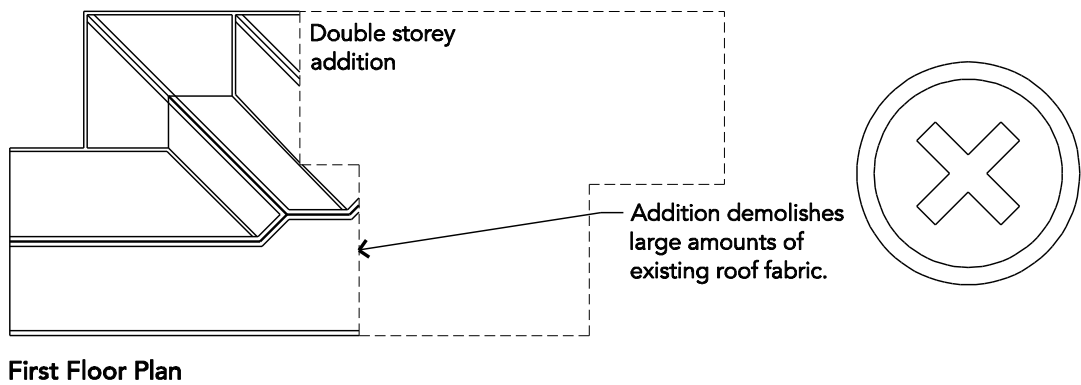
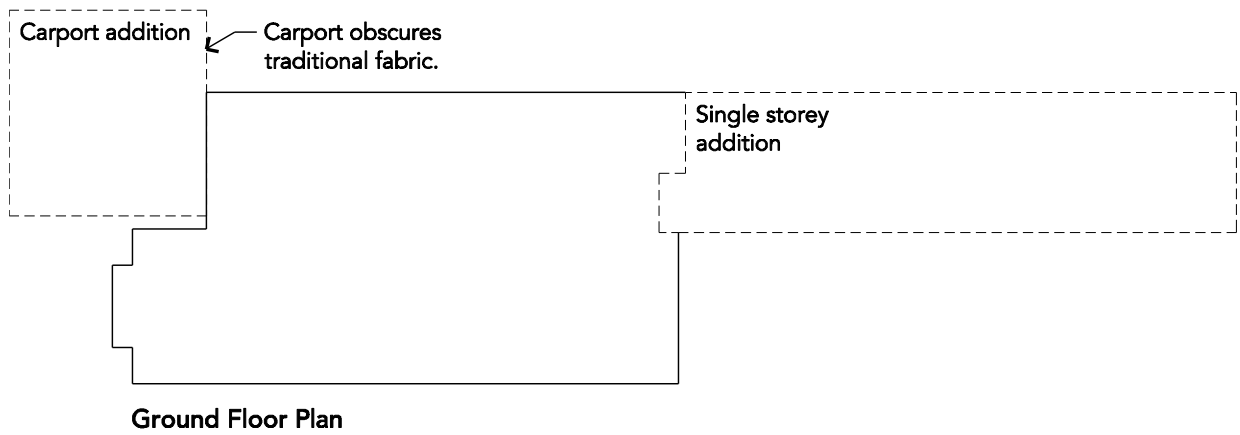


Figure 9 - Plans and elevations illustrating incompatible alterations and additions - *Griffiths Architects 2012.*

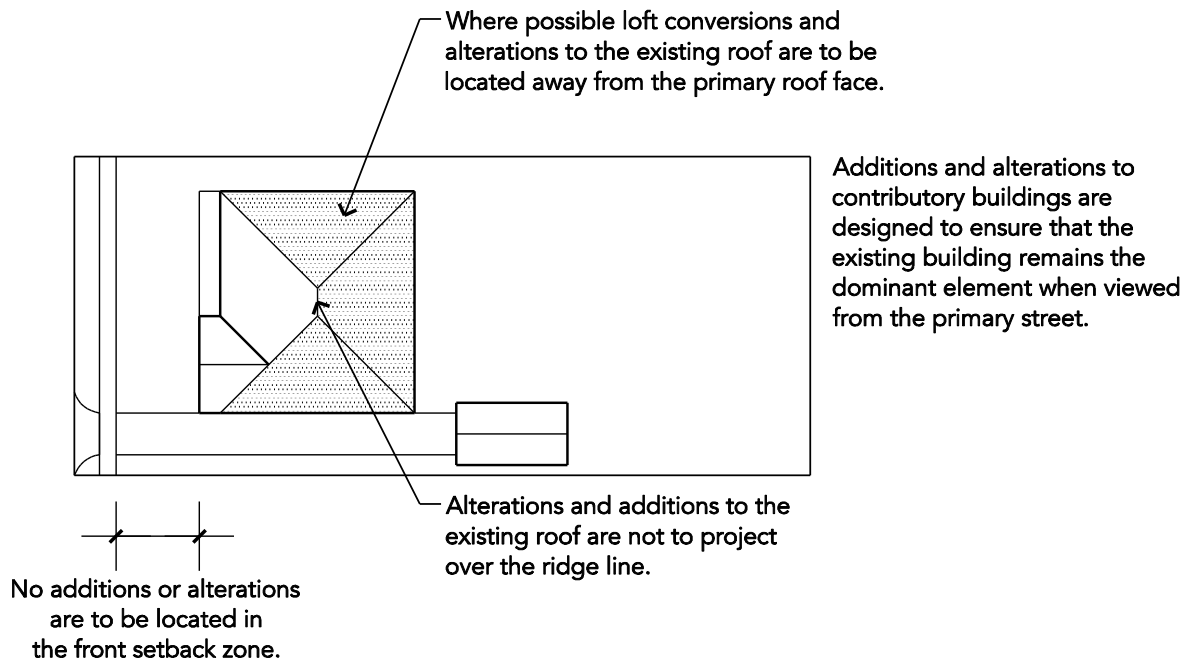


Figure 10 – Plan illustrating compatible and incompatible roof conversions – *Griffiths Architects 2012*.

### 3.7.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1.1</b> Additions and alterations to contributory buildings are designed to ensure that the existing building remains the dominant element when viewed from the primary street and to ensure that the existing building’s contribution to the streetscape is maintained. The council shall allow additions to be located in the front setback zone where there is no other option and the addition is demonstrably compatible with the existing streetscape character and not impact on the heritage value of a particular place. All applications to include site plans, plans and street elevations.</p>	<p><b>A1.1</b> Single storey additions located behind the primary dwelling and not visible from the primary street.</p> <p><b>A1.2</b> Second storey additions that are:</p> <ul style="list-style-type: none"> <li>i. Accommodated within the existing roof (without changes to the roof geometry); and,</li> <li>ii. Built behind the existing building and not visible from the opposite side of the street. A minor variation to this may be permitted on the basis of its impact on the streetscape.</li> </ul>
<p><b>P1.2</b> Replacement of, or construction of, elements such as carports shall not obscure the original dwelling.</p>	
<p><b>P2</b> Alterations to openings, or new openings, facing the street to have a vertical profile (refer glossary) or to be composed of vertical modules.</p>	<p><b>A2</b> New openings to primary façade to be of a vertical proportion.</p>

### 3.7.3 Development of Existing Buildings

For specific provisions regarding additions and alterations to existing buildings refer section *Additions and Alterations*.

#### 3.7.3.1 Statement

*The Policy Area retains a large amount of original building fabric in the Plympton, Woodside, Richmond and Riverside Precincts. Richmond Hill retains a small amount of original building fabric. The Richmond Raceway and Preston Point Precincts contain the newest developments in the Policy Area. Overall the Policy Area has a high heritage value. This value depends heavily on the conservation and maintenance of its heritage fabric. Many of the traditional residences in the Policy Area have been conserved, adapted or altered in a sympathetic manner.*



#### 3.7.3.2 Desired Outcomes

- i. Retention of original building fabric;
- ii. Restoration of contributory buildings to their original or earlier form. This should be based on historical evidence where possible;
- iii. Removal of intrusive elements to contributory buildings;
- iv. Streetscape presence of heritage buildings to be retained and not have their prominence reduced by additional works when viewed from the street or significant vantage points such as from the river;
- v. Updating of services to existing buildings with the least amount of change to the fabric; and,
- vi. Replacement of deteriorated building fabric to match original detail.

### 3.7.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Additions and alterations to contributory buildings to retain as much original building and its fabric as possible and also to retain historically significant additions.</p>	<p><b>A1</b> Relevant drawings to illustrate impact of additions and alterations on heritage fabric. This shall include existing and proposed plans, elevations and possible sections. Existing photos shall also be included.</p>
<p><b>P2</b> Restoration of contributory buildings to their original or earlier form.</p>	<p><b>A2</b> Provide relevant historic information to support proposed restoration and/or reconstruction. This shall include historic plans, elevations and photos where possible.</p>
<p><b>P3</b> Streetscape presence of heritage buildings to be retained.</p>	<p><b>A3</b> Relevant drawings to illustrate streetscape presence of heritage building to be retained. This shall include existing and proposed site plans and street elevations. Street elevations to include a minimum of the subject lot and two neighbouring lots.</p>
<p><b>P4.1</b> Replacement of deteriorated building fabric with either:</p> <ul style="list-style-type: none"> <li>i. A 'like for like' approach; or,</li> <li>ii. In a similar but not identical fashion where appropriate.</li> </ul>	<p><b>A4</b> Replacement of corrugated iron roofing with Galvanised steel.</p>
<p><b>P4.2</b> Example of material replacement:</p> <ul style="list-style-type: none"> <li>i. Replacement of corrugated iron roofing with Colorbond, Galvanised steel or Zinalume roof sheeting. On places of high significance, Galvanised steel is the preferred option, laid in short length sheets.</li> </ul>	



### 3.7.4 Site Works

#### 3.7.4.1 Statement

*The Policy Area has a distinct undulating landscape. Residential construction varies throughout the Area due to the undulating nature of the topography. A number of residences are constructed above and below the level of the road depending on the topography. Residences however are generally sited consistently on their lots and provide a high level of visual coherence. Generally heights of residences on sloping streetscapes are evenly staggered.*



#### 3.7.4.2 Desired Outcomes

- i. New developments are to be sited to reflect the immediate locality to retain a high level of visual coherence in streetscapes;
- ii. New developments are to maintain the prevailing natural ground level (refer glossary) of the site and to minimise the need for cutting and filling of the site and boundary retaining walls; and,
- iii. New developments, additions and alterations are to be designed so that a strong level of visual privacy is retained.

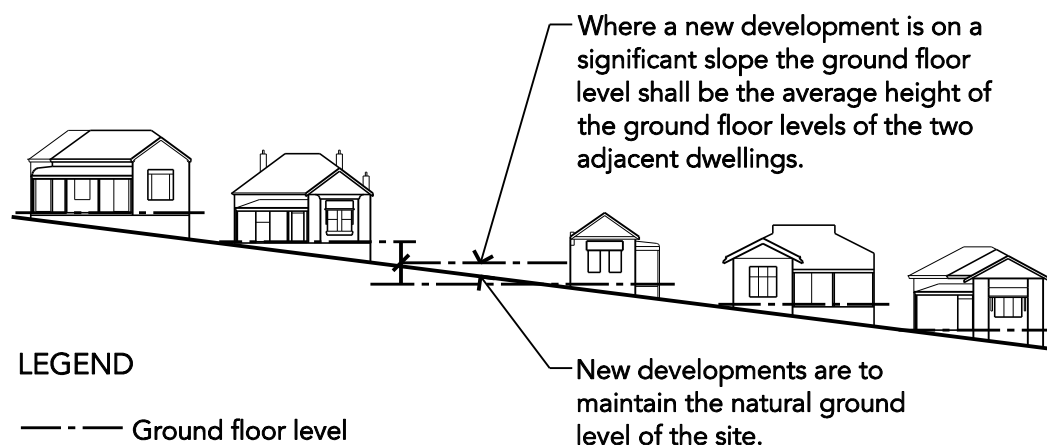


Figure 11 – Street elevation illustrating ground floor level for new developments – *Griffiths Architects 2012.*

### 3.7.4.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Siting of new developments is to be consistent with the immediate locality and shall not negatively impact on the streetscape character and amenity.</p>	<p><b>A1</b> Dwellings are to be constructed on a plinth (refer glossary) limited in area to, and within the boundaries of, the fully covered roof area.</p>
<p><b>P2</b> New developments are to maintain the prevailing natural ground level of the site.</p>	<p><b>A2.1</b> Where a new development is on a significant slope the ground floor level shall be the average height between the ground floor levels of the two adjacent dwellings.</p>
	<p><b>A2.2</b> Where there are no adjacent dwellings the ground floor level shall be located to minimise the impact on the need for cutting and filling of the site without adversely impacting the streetscape. This is to be demonstrated by a drawing(s) including site section and street elevation. Street elevations are to be a minimum of the subject lot and two neighbouring lots.</p>
<p><b>P3</b> New developments, additions and alterations are to be designed so that a strong level of visual privacy is retained.</p>	<p><b>A3</b> Refer R Codes for acceptable development provisions on visual privacy.</p>

### 3.7.5 Demolition

#### 3.7.5.1 Statement

*The Policy Area retains much of its original building fabric. The demolition of traditional buildings has been minimal. Traditional buildings contribute significantly to the character of the Policy Area and are vital to the retention of heritage values of the town. Therefore the demolition of heritage places, part demolition of individually contributory buildings (refer glossary) and the demolition of contributory elements is discouraged. However the removal of inappropriate alterations, additions and works that detract from the cultural significance of the place and its proper presentation is encouraged.*

#### 3.7.5.2 Desired Outcomes

- Conservation of significant and contributory places; and,
- The removal of inappropriate alterations, additions and works that detract from the cultural significance of the place.

### 3.7.5.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1.1</b> Demolition of any building or place on the Heritage List requires the development approval of the Town. The Town shall not approve demolition of a building until there is a planning approval granted for a new building that complies with this Local Planning Policy. Where no other development is proposed the owner of the listed building will be requested to give reasons for the planning application for demolition so that Council shall be fully informed in its decision-making.</p>	<p><b>A1.1</b> Relevant information of new development, additions or alterations submitted with planning application for demolition.</p> <p><b>A1.2</b> Relevant drawings illustrating the demolition area and zones of significance (refer glossary) of the existing building. This is to include demolition plans and elevations and possible sections. Street elevations are to be included including a minimum of the subject lot and two neighbouring lots.</p>
<p><b>P1.2</b> The Local Government shall not approve demolition if the proposed development, addition(s) and/or alterations do not conform to this Local Planning Policy.</p>	
<p><b>P2</b> Applications for demolition of all or part of an existing contributory building must demonstrate the cultural heritage significance of the existing building and the proposed demolition area(s) are not adversely affected by the demolition.</p>	<p><b>A2</b> For contributory buildings a comprehensive Development Impact Statement that addresses the implications of the demolition is to be provided. An acceptable format is attached in Appendix A.</p>
<p><b>P3.1</b> Planning application for demolition will not be approved for a contributory building on the grounds of neglect, poor condition or economic/other gain for redevelopment of the land.</p>	<p><b>A3</b> If a place is included on the Municipal Inventory, in a designated heritage area, or included a TPS Schedule of heritage places, relevant information is required from suitably qualified consultants, such as a structural engineer or heritage architect, to assess demolition applications.</p>
<p><b>P3.2</b> Part demolition of individually significant or contributory buildings or removal of contributory elements will not be approved unless it is demonstrated to the Council's satisfaction:</p> <ul style="list-style-type: none"> <li>i. that part of the heritage place has been changed beyond recognition of its original or subsequent contributory character(s),</li> <li>ii. that part is not visible from a street frontage, park or public open space and the main building form including roof form is maintained,</li> </ul>	

- iii. the removal of the part would not adversely affect the contribution of the building to the heritage place, and
- iv. for individually significant building or works, it can be demonstrated that the removal of part of the building does not negatively affect the significance of the place.

**NOTE:** Refer Town of East Fremantle Town Planning Scheme for developments that require planning approval.

### 3.7.6 Construction of New Buildings

#### 3.7.6.1 Statement

*The building fabric of the Policy Area is from the late 1890s to the present day. The successive layers of buildings and styles contribute to the significance and character of the Policy Area.*

*New developments should respect the immediate locality and should not replicate traditional building forms. Faux (refer glossary) or imitation heritage buildings should be avoided. Generally this type of design is unconvincing in comparison to the original historic types and devalues the authentic buildings. Designs that do not follow the established pattern in terms of form, scale, bulk, or other design features shall not be supported.*

*Where relevant, viewsheds may be significant and a study may be required by Council to demonstrate compatibility with viewsheds. Precincts where this is most likely to be a requirement include Riverside, Preston Point and Richmond Hill north abutting Preston Point Road.*

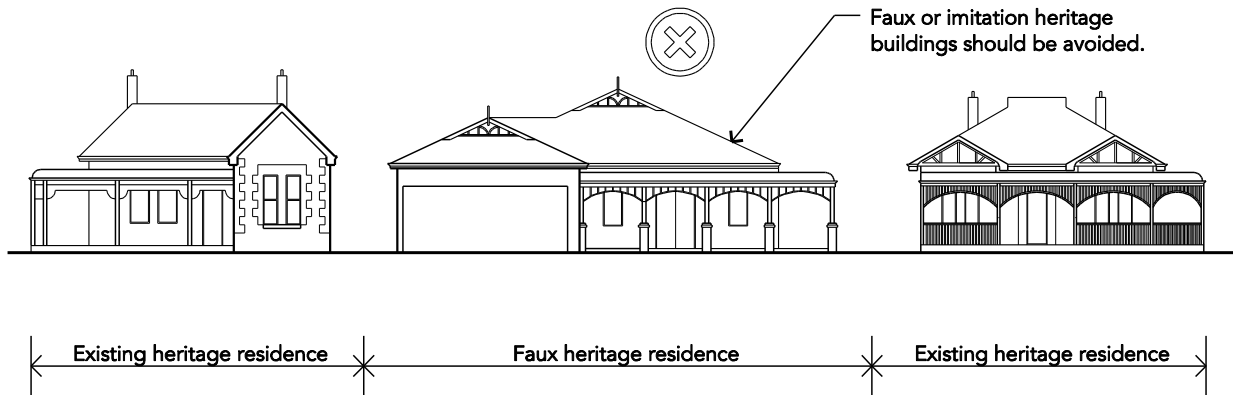


#### 3.7.6.2 Desired Development Outcomes

- i. New developments should not replicate traditional building form (refer note 2 below);
- ii. New developments should respect and reflect the established development pattern; and,
- iii. New developments should not negatively impact on the streetscape character.

**NOTE 1:** Applications for planning approval for new buildings are to comply with all other design elements of this LPP.

**NOTE 2:** For example where the immediate locality is of traditional built form, such as Interwar Californian Bungalows, new developments should be of a contemporary style without traditional influences. New developments however, should reflect the traditional setbacks, scale and bulk of the traditional buildings.



**Figure 12** – Street elevation illustrating incompatible faux heritage development – *Griffiths Architects 2012*. Faux styles devalue the authentic places. Contemporary requirements associated with new homes, results in faux heritage not having the same qualities as authentic places.

### 3.7.6.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>All Dwellings</b></p> <p><b>P1</b> New buildings are to be designed and constructed in a style compatible with, but which does not overtly mimic, the traditional building styles found in the Town.</p> <p><b>P2</b> New buildings shall be compatible with and not adversely impact the view sheds in the Town.</p>	<p><b>All Dwellings</b></p> <p><b>A1</b> Developments to comply with all design elements of this Local Planning Policy and are compatible with the context in terms of bulk, scale, materials and design.</p> <p><b>A2</b> Relevant information demonstrating development compatibility with existing view sheds. This is to include a photo montage of the proposed development within the existing viewshed.</p>

### 3.7.7 Building Setbacks and Orientation

#### 3.7.7.1 Statement

*The Policy Area has a predominantly grid-like pattern with residences addressing the street. Buildings in the Precincts of Richmond Hill Riverside and Preston Point are also orientated to the Swan River where possible. Setback patterns differ in each precinct but are predominantly consistent. The pattern of setback is critical to the character of each Precinct. While there are some variations between streets, there is little opportunity for development within the street setback area. Side setbacks are generally consistent within each Precinct.*



### 3.7.7.2 Desired Development Outcomes

- i. No additions to existing dwellings shall be constructed within the front setback area; and,
- ii. New developments are to be constructed with front and side setbacks to match the traditional setbacks of the streetscapes.

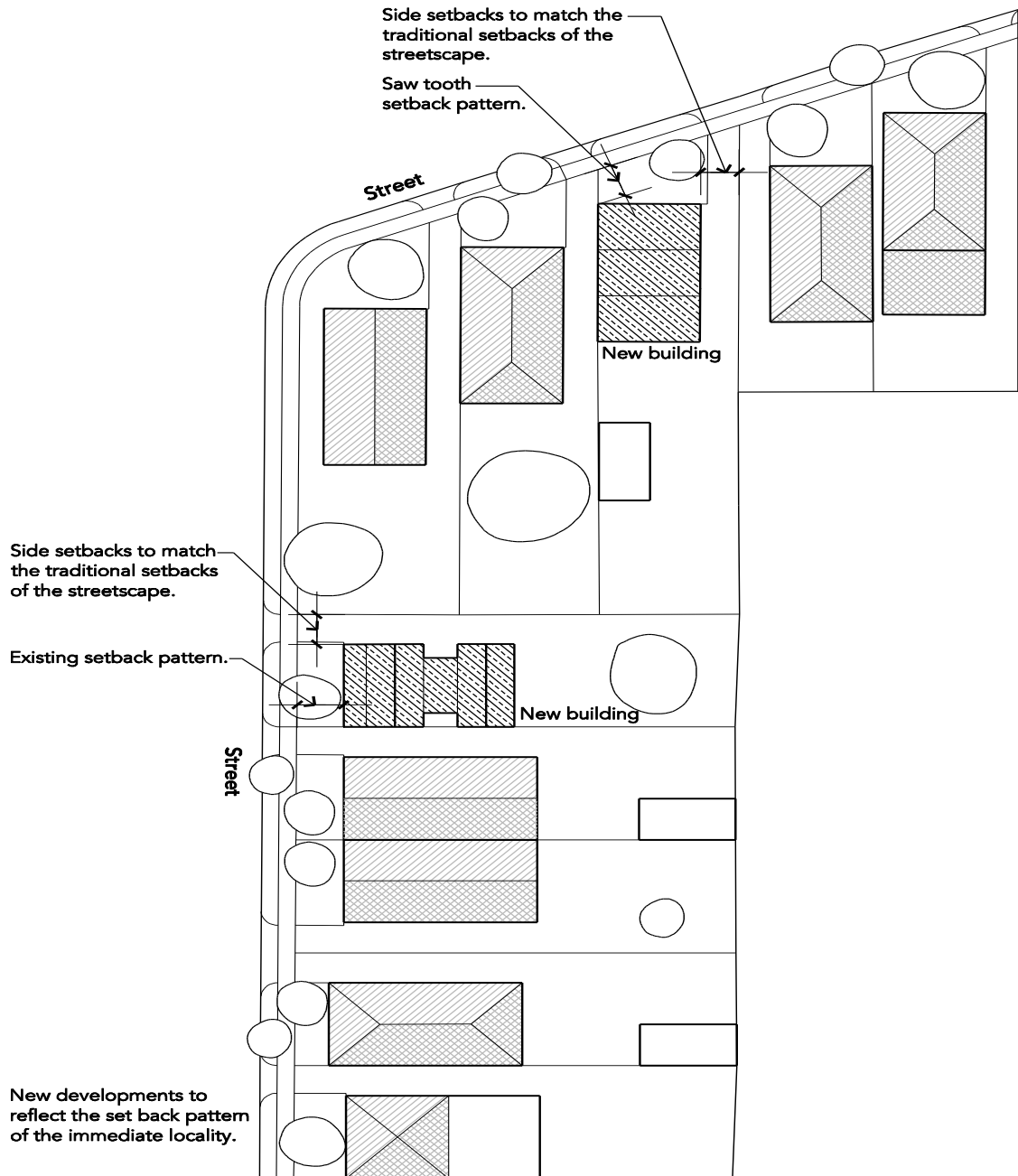


Figure 13 – Site plans illustrating existing setback patterns and compatible new developments – *Griffiths Architects 2012*.

### 3.7.7.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1.1</b> The primary street setback of new developments or additions to non-contributory buildings is to match the traditional setback of the immediate locality.</p>	<p><b>A1.1</b> New developments, additions and alterations are to match the existing front and side setbacks of the immediate locality.</p>
<p><b>P1.2</b> Additions to existing contributory buildings shall be setback so as to not adversely affect its visual presence.</p>	<p><b>A1.2</b> Additions to contributory buildings are to be setback a minimum of 100mm from the primary frontage of the existing building.</p>
<p><b>P1.3</b> Developments are to have side setbacks complementary with the predominant streetscape.</p>	<p><b>A1.3</b> Where instances of front setbacks of adjacent residences vary, the front setback of the new development shall either:</p> <ul style="list-style-type: none"> <li>i. Match the front setback of one existing dwelling;</li> <li>or,</li> <li>ii. Be the average of the two setbacks</li> </ul>
<p><b>P2</b> Developments to be orientated to address the street.</p>	<p><b>A2</b> Developments to be at a right angle to the street.</p>
<p><b>P3</b> Wall heights to reflect the immediate locality.</p>	<p><b>A3</b> A wall may be situated closer to an adjoining residential boundary than the standards prescribed in Tables 1, 2a or 2b of the Residential Design Codes where the following are observed:</p> <ul style="list-style-type: none"> <li>i. Walls are not higher than 3m and up to 9m in length up to one side boundary;</li> <li>ii. Walls are behind the main dwelling;</li> <li>iii. Subject to the overshadow provisions of the Residential Design Codes – Design Element 6.9;</li> <li>iv. In the opinion of the Council, the wall would be consistent with the character of development in the immediate locality and not adversely affect the amenity of adjoining property(s) having regard for views; and,</li> <li>v. Where the wall abuts an existing or simultaneously constructed wall of similar or greater dimensions.</li> </ul>

**P4** Where river views are possible, new developments are to be orientated towards the river.

**A4** Orient development towards the river.

### 3.7.8 Roof Form and Pitch

#### 3.7.8.1 Statement

*The Town's roof forms are predominantly gable, gambrel and hipped. Roofs have overhanging eaves and are pitched between 28° and 36° (approx.). The council shall approve contemporary roof forms, including skillion, flat roofs or parapet, on new developments where demonstrated to be compatible with the existing and surrounding residences.*



#### 3.7.8.2 Desired Development Outcomes

- i. Retention of the traditional pattern of development with consistent roof forms;
- ii. Contemporary roofs to the Richmond Hill and Riverside Precincts;
- iii. New developments with contemporary roof forms that are compatible with the immediate locality; and,
- iv. Roof forms of additions and alterations that positively contribute to the existing dwelling (refer note below).

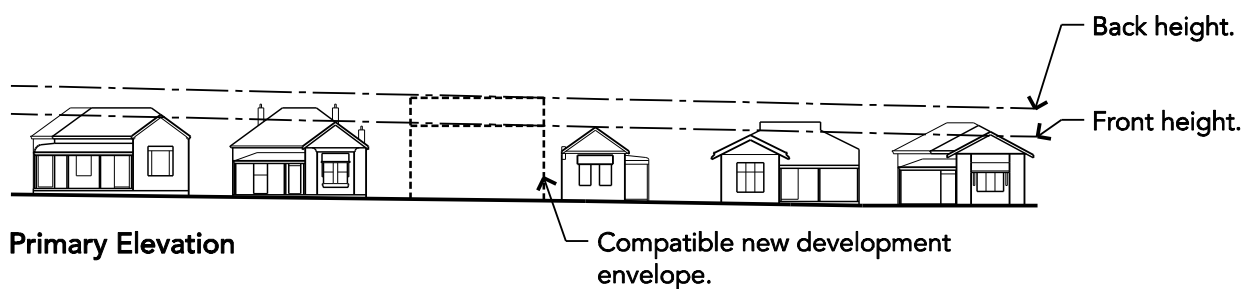


Figure 14 - Street elevation illustration compatible roof envelopes - Griffiths Architects 2012.



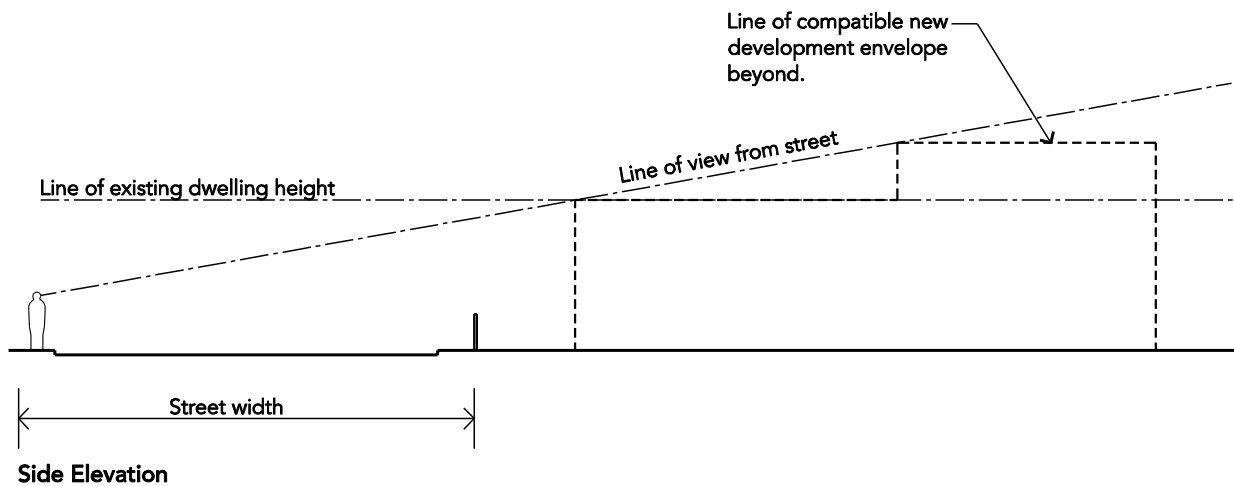


Figure 15 – Side elevation illustrating compatible roof envelope – *Griffiths Architects 2012*.

NOTE: Acceptable roof forms for addition to existing houses may be, but are not limited to the following:

- i. A lean-to roof that sits below the roof line of an existing hipped roof; and,
- ii. An existing dwelling with a gable roof could have a rear addition with a skillion roof that reflects the existing dwelling.

### 3.7.8.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>All Precincts</b></p> <p><b>P1</b> Roof forms of additions and alterations positively contribute to the existing dwelling.</p> <p><b>P2</b> The eaves of alterations and additions are to complement the eaves of the existing building.</p> <p><b>P3</b> The eaves of new developments are sympathetic with immediate locality in regard to size of overhang.</p> <p><b>Woodside, Richmond, Plympton and Raceway</b></p> <p><b>P4</b> Roof forms of new buildings complement the traditional form of surrounding development in the immediate locality.</p>	<p><b>All Precincts</b></p> <p><b>A1</b> Additions and alterations to a contributory building are to match the original roof pitch.</p> <p><b>A2</b> Eaves to new developments, additions and alterations are unlined.</p> <p><b>A3</b> Size and overhang of eaves to new developments, additions and alterations to match the immediate locality.</p> <p><b>Woodside, Richmond, Plympton and Raceway</b></p> <p><b>A4.1</b> Roof forms of new developments should be pitched between 28° and 36° and are of consistent scale and form with the prevailing building typology in the immediate locality.</p> <p><b>A4.2</b> A contemporary roof form or roof pitch that is less than 28° or greater than 36° shall be approved where the</p>

### Richmond Hill and Riverside

**P5** Roof forms not to be restricted to traditional roof forms. Roof forms are to not adversely affect the immediate locality.

applicant demonstrates compatibility with the immediate locality.

**A4.3** Chimney(s) of contributory buildings are retained.

### Richmond Hill and Riverside

**A5** Relevant information demonstrating impact of roof on the immediate locality to be provided. This is to include site plans, roof plans, elevations and sections. Photos of the streetscape should also be provided. Street elevations are to be included including a minimum of the subject lot and two neighbouring lots.

## 3.7.9 Materials and Colours

### 3.7.9.1 Statement

*Residences in the Policy Area are predominantly of timber, brick and limestone construction with corrugated iron and Marseille tiled roofs. Face brick, rendered brick and painted brick finishes are evident throughout the Policy Area. Some corrugated iron roofs have been replaced with Colorbond or Zinalume.*

*For a specific existing material palette for each Precinct refer to the Town of East Fremantle Precinct Survey.*

*External face brick or stone walls are defining elements or characteristics of a building and should not be coated, rendered or painted. Full authentic restoration of original colour schemes is not required in the Policy Area. Original cladding materials should not be removed. Where repairs or replacement are necessary, this should be undertaken using materials consistent with the existing fabric.*

**NOTE:** Asbestos removal should be carried out in accordance with statutory regulations.



### 3.7.9.2 Desired Development Outcomes

- i. For existing contributory buildings, retain original materials where possible. Where materials require replacement, the policy of replacing 'like for like' should be applied;

- ii. External colour schemes to existing buildings should be appropriate to the architectural period and style of the building, based on historical evidence where possible;
- iii. New materials and colours that are compatible but distinguish the addition and alteration from the existing building are preferred; and,
- iv. New materials should be easily distinguishable from existing materials without detracting from the character and heritage significance of the Precinct.

### 3.7.9.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>Existing Buildings:</b></p> <p><b>P1</b> Where possible existing materials should be retained. For contributory buildings where original materials require replacement, the policy of replacing 'like for like' should be applied.</p> <p><b>P2</b> Replacement of existing materials with new materials shall be approved if demonstrably compatible with the immediate locality.</p> <p><b>P3</b> Reinstatement of original colour is encouraged for contributory buildings. Where possible this should be informed by historical evidence including photos and paint scrapes.</p> <p><b>Additions and Alterations:</b></p> <p><b>P4.1</b> Materials and colours to additions and alterations should either match the original or be compatible with the immediate locality. New materials and colours that are compatible but distinguish the addition and alteration from the existing building are preferred.</p>	<p><b>Existing Buildings:</b></p> <p><b>A1</b> Existing face brick and stonework is retained.</p> <p><b>A2.1</b> Original cladding materials should not be removed. Where repairs or replacement is necessary, this should be undertaken using materials consistent with existing fabric.</p> <p><b>A2.2</b> Existing timber joinery retained or replaced with new timber joinery to match existing where required.</p> <p><b>A2.3</b> Existing roof material is retained or replaced as required. Replacement materials should match existing for contributory buildings. New materials will have to be demonstrated as compatible with the immediate locality.</p> <p><b>A3</b> Retain or reinstate original colour(s) of the residence.</p> <p><b>Additions and Alterations:</b></p> <p><b>A4.1</b> For alterations to existing dwellings the materials should match the materials of the original dwelling.</p> <p><b>A4.2</b> For additions to existing dwellings the materials should be compatible but distinguishable from the existing. This to be demonstrated in relevant elevations.</p>

**P4.2** Roofs of alterations or additions should be clad in materials that do not detract from the built form of the existing dwelling.

**New Developments:**

**P5** Materials incorporated in to new developments are to be compatible with the colour and finishes of existing materials in the immediate locality.

**New Developments:**

**A5** Assessment will be case-by-case based on the Performance Criteria.

**3.7.10 Landscape Guidelines**

**3.7.10.1 Statement**

*Established vegetation makes a valuable contribution to the amenity of the community, in particular with respect to shade, privacy, aesthetics and their contribution towards minimising greenhouse gases, maintaining ground water levels and providing habitats for birds and fauna. The retention of existing trees, shrubs and hedges on private property is encouraged. Development proposals should avoid the removal of, or taking of any action which will damage, existing mature trees on the site.*

**3.7.10.2 Desired Development Outcomes**

- i. Established vegetation, particularly mature trees, shrubs and hedges, is to be retained.
- ii. Landscaping is to be compatible with the character of the immediate locality.

**3.7.10.3 Performance Criteria and Acceptable Development Provisions**

Performance Criteria	Acceptable Development Provisions
<p><b>Existing Buildings:</b></p> <p><b>P1</b> Development shall be designed around significant established vegetation.</p>	<p><b>Existing Buildings:</b></p> <p><b>A1.1</b> Development applications are to be accompanied by a site survey including the location of existing mature trees, shrubs, hedges and other significant vegetation.</p> <p><b>A1.2</b> Any proposal to remove existing significant vegetation shall be accompanied by a tree survey justifying removal of the vegetation and prepared by a suitably qualified consultant or by a landscape plan demonstrating compensating re-vegetation of the site.</p>



### 3.7.11 Front Fences

#### 3.7.11.1 Statement

*There is a variety of fencing styles in the Policy Area that contribute to its overall character. Front fences are predominantly low, permeable and reflect the period of the individual residence. Traditional fence types are a combination of brick and brick piers (face brick, painted and rendered brick finishes), limestone, timber (painted and sealed), steel frame and wire mesh. Heights range between 600mm and 1200mm. Some areas of Richmond Hill have no front fences. Front lawns in this area often transition into verges seamlessly.*

*Well designed contemporary front fences are evident in many Precincts (refer photos below). Contemporary materials and forms may be explored in fences without detracting from the streetscape. There are positive examples of new fences that fit comfortably within the Precincts.*

#### 3.7.11.2 Traditional Fence Types



37 Hamilton Street



25 Osborne Road



117 George Street



27 Windsor Road

### 3.7.11.3 Contemporary Fences



30 Hubble Street



63 Richmond Circus

### 3.7.11.4 Desired Development Outcomes

- i. Where an original fence remains it should be retained and restored where required. In the event an original fence requires replacing a 'like for like' approach should be encouraged;
- ii. Fences are no higher than 1.8m;
- iii. Permeable front boundary fences;
- iv. Hedges are permitted and reinstatement should be encouraged; and,
- v. Fences should be designed to complement the design of the existing dwelling. Both traditional and contemporary fence styles can be appropriate.

### 3.7.11.5 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Contributory existing fences should be retained and restored where required.</p>	<p><b>A1</b> Retain existing contributory fence (refer glossary) or restore where required with a 'like for like' approach.</p>
<p><b>P2</b> Fences along the street boundary are low and complement the individual building and the immediate locality. Fences should demonstrate the following:</p> <ul style="list-style-type: none"> <li>i. Appropriate height, material and colour;</li> <li>ii. Adequate sight lines around intersections for pedestrian and vehicles;</li> <li>iii. Complies with requirements under the Residential Design Codes; and,</li> <li>iv. Maximum height of 1.8m to the satisfaction of the Local Government.</li> </ul>	<p><b>A2.1</b> Appropriate materials for fence construction are as follows:</p> <ul style="list-style-type: none"> <li>i. Timber pickets;</li> <li>ii. Woven wire;</li> <li>iii. Link mesh;</li> <li>iv. Brick or stone of maximum height 1.2m with piers to 1.8m, with infill of timber pickets or metal railings; or,</li> <li>v. Hedges where they are maintained to a height that allows passive surveillance of the street.</li> </ul>
	<p><b>A2.2</b> Materials not included in the above list shall be approved by council where demonstrably compatible with the immediate locality. All submissions are to include a street elevation with a minimum of the subject lot and two adjacent properties.</p>
<p><b>P3</b> Fences above 1.2m are to be visually permeable to allow passive surveillance to the street.</p>	<p><b>A2.3</b> Relevant information illustrating fence construction. This is to include photos and a street elevation of the immediate locality. Street elevations are to include a minimum of the subject lot and two neighbouring lots.</p> <p><b>A3</b> Fence area above 1.2m should be at least 60% permeable. Permeability is to be evenly distributed across fence length and area.</p>
<p><b>P4.1</b> Less permeable fences above 1.2m may be approved when they meet the following:</p> <ul style="list-style-type: none"> <li>i. A higher fence/wall is required for noise attenuation;</li> <li>ii. A less permeable fence would aid in reducing, headlight glare from motor vehicles. This would apply more particularly where the subject is opposite or adjacent to an intersection which could</li> </ul>	<p><b>A4</b> Demonstration of reasons for requiring a less permeable fence in regards to the performance criteria.</p>

lead to intrusion of light into windows of habitable rooms;

- iii. Where the contours of the ground or the difference in levels between one side of the fence and the other side warrant consideration of a higher fence;
- iv. Where the applicant can demonstrate to the Council that there is a need to provide visual screening to an outdoor living area. This shall apply in situations where there is no alternative private living space other than in the front of the residence or for part of the secondary side boundary of a corner lot.

**P4.2** Where a lot has frontage to two streets a fence/wall shall not be constructed within the first 6m of the secondary frontage from the primary frontage unless it complements the design of the fence/wall along the primary frontage.

**P4.3** A person shall not erect any fence/wall or structure within a 6m truncation of intersecting road reserves (or their prolongation where a truncation has already been set aside) to a height greater than 0.75m unless approval of the Council has been obtained in writing.

**NOTE:** The height of a fence is defined as the vertical distance between:

- i. The top of the fence at any point; and,
- ii. The ground level immediately below that point. Where the ground levels on each side of the fence are not the same the lower ground level should be chosen.

### 3.7.12 Pergolas

#### 3.7.12.1 Statement

*Pergolas are not commonly visually evident within the streetscapes of the Policy Area. Pergolas are generally located to the rear or side of residences and are not dominant features on the streetscape.*





### 3.7.12.2 Desired Outcomes

- i. Pergolas are designed and located where possible to minimise their visual impact on the character of the building and the immediate locality;
- ii. Pergolas are to be incorporated within landscaping of residences and designed to reflect the existing dwelling; and,
- iii. Pergolas construction method not to be detrimental to the existing building fabric.

### 3.7.12.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<b>P1</b> Pergola construction to the standards of the Building Code of Australia and Australian Standards.	<b>A1</b> Plans and construction details are submitted and a planning approval is obtained (Refer the Town of East Fremantle Town Planning Scheme for planning approval requirements).
<b>P2</b> Landscape design to be sympathetic to the natural ground level.	<b>A2</b> Landscape to be terraced.
<b>P3</b> Roofing to pergolas to be permeable.	<b>A3</b> Pergolas to be roofed with shade cloth.
<b>P4</b> Pergola construction method to not be detrimental to the existing building fabric.	<b>A4</b> Relevant information from suitably qualified consultants, such as a structural engineer or heritage architect, to assess the impact of the pergola on the existing building fabric of a building on the MHI and is proposed to be built in the front setback.
<b>P5</b> Pergolas supports are to provide sufficient clearance from side and rear boundaries.	<b>A5</b> Pergola supports to be a minimum of 1200mm away from side and rear boundaries.
<b>P6</b> The size and scale of a pergola is to not visually dominate the existing building or obstruct established views from adjacent properties.	<b>A6.1</b> Relevant drawings to illustrate visual presence of pergola on the existing building and immediate locality. <b>A6.2</b> Pergolas are not to be located within the front setback area. The council shall approve pergolas in the front

**P7** Pergolas are designed so as:

- i. Not to create a traffic hazard;
- ii. Not to be constructed over underground services; and,
- iii. Not to be constructed under power lines.

**P8** Pergolas proposed to be erected on corner lots are to be determined by Council which will approve the erection of such structures only after it is satisfied that the proposal meets all the requirements by other Statutory Authorities and creates no additional hazards, does not interfere with views and complies with the setbacks and truncations required under the Metropolitan Region Scheme and the Town Planning Scheme.

setback area where the applicant can demonstrate that pergolas in the front setback zone are an established feature of the streetscape. Dwellings deemed of high heritage value shall not have pergolas in the front setback area.

**A7** Relevant drawings to illustrate position of pergola in relation to power lines, underground services and potential traffic.

**A8** All construction materials must be described on applicant's drawings.

### 3.7.13 Incidental Development Requirements

#### 3.7.13.1 Statement

*The purpose of this section is to protect the quality of streetscapes and the amenity of nearby residents by minimising the visual impact of structures such as satellite dishes, microwave antennae, television antennae, air conditioners, solar collectors, rainwater tanks and tower masts.*

*The above fixtures shall be erected in such a manner as to minimise the visual impact on the subject site, streetscape and immediate locality in terms of design, scale, bulk, colour, height and general appearance. The location of the aforementioned fixtures shall not have an adverse impact on the architectural integrity of the existing building and the immediate locality.*



### 3.7.13.2 Desired Development Outcomes

- i. Incidental items such as television aerials, satellite dishes, solar panels are to be located away from the primary street in order to minimise their impact on contributory buildings and the character of Policy Area.

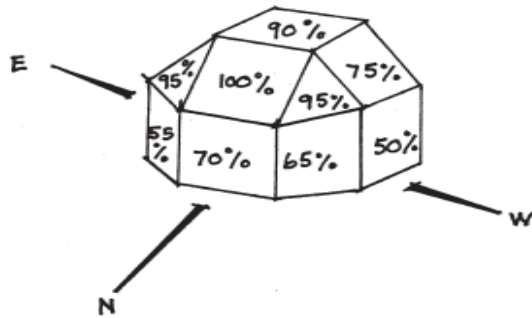


Figure 16 – Percentage of power theoretically generated by solar arrays. *Colin Mendoza, Solar Power Brochure. BP Solar and Solar Partners.*

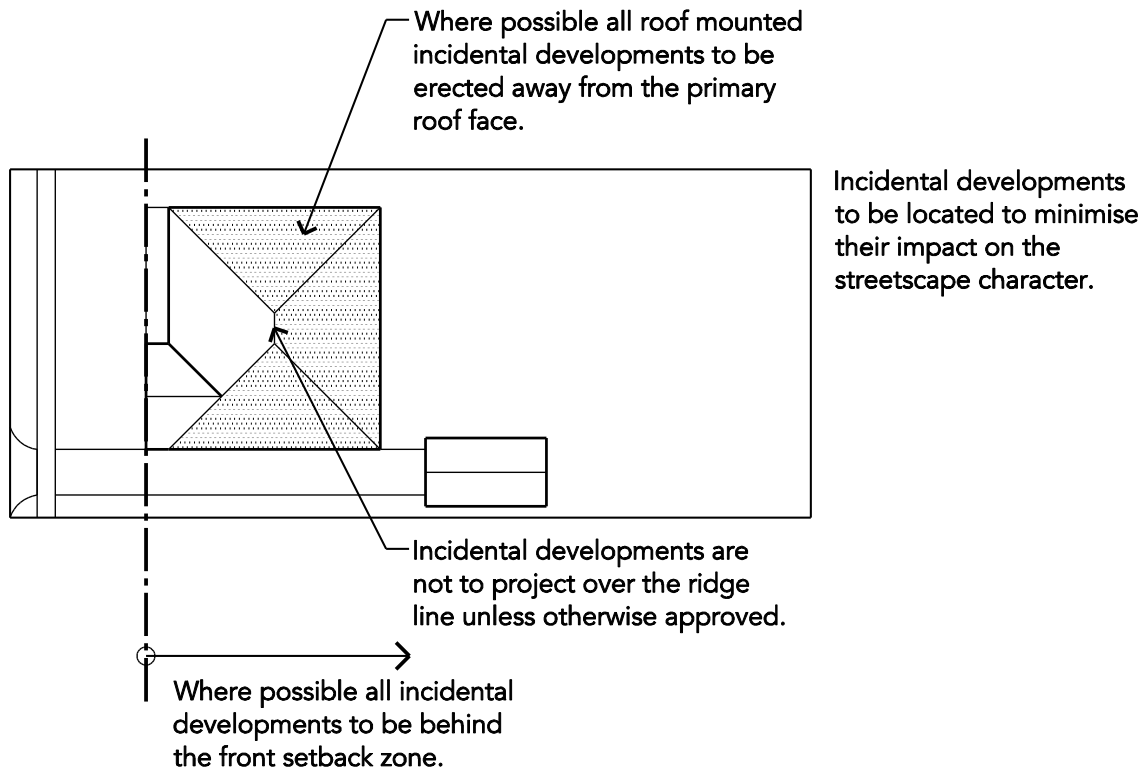


Figure 17 – Location of incidental developments – *Griffiths Architects 2012.*

### 3.7.13.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Incidental development such as solar collectors, water tanks, satellite dishes, microwave and radio masts, air conditioners and TV antennae are located to minimise their impact on the character of the building and the immediate locality. Wherever practical, opportunities for concealment of incidental developments shall be utilised – such as ‘hiding’ them in roof valleys, on rear roof planes or behind parapets.</p>	<p><b>A1.1</b> Solar collectors, satellite dishes, microwave masts, radio masts and antennae are not to be visible from the primary street.</p> <p><b>A1.2</b> Rainwater tanks should:</p> <ul style="list-style-type: none"> <li>i. Be behind the building line and not visually dominate the building as viewed from the street;</li> <li>ii. Not exceed 5000L capacity (above 5000L requires a planning approval);</li> <li>iii. Set 1m away from any boundary;</li> <li>iv. Be constructed and retained to the relevant Australian Standard;</li> <li>v. Have an overflow that is directed to a soak well within the property; and,</li> <li>vi. Not exceed a height of 2.4m from the ground floor level of the dwelling.</li> </ul> <p><b>A1.3</b> The diameter of a satellite dish is to be less than 1.2m. Where the diameter of a satellite dish is greater than 1.2m but less than 1.5m it shall be permitted if it can be demonstrated that it does not adversely impact on the visual amenity of the adjacent area. Where the diameter of a satellite dish is greater than 1.5m they are to be located at ground level and have a maximum height of 3m from natural ground level and not be visible from the street.</p> <p><b>A1.4</b> The erection of a tower mast for the transmission and reception of radio signals in excess of 5 metres from site datum and/or natural ground level is not permitted unless no written objections are received by the Town and the owners and occupants of the affected properties.</p> <p><b>A1.5</b> Microwave antennae to have an area less than 0.6m<sup>2</sup> and to not project higher than 3m above the ridge line.</p>

**P2** All solar panels shall be positioned to avoid existing shadows cast from nearby buildings, structures and trees.

**P3** Incidental developments are to be constructed behind the front setback zone.

**P4** Incidental developments are not to project above the ridge line of the building except for TV antennae.

**P5** The historic fabric of a building should not be unnecessarily disturbed or destroyed, in line with minimum intervention and reversibility principles. For example when a system is removed the building should be able to be fully restored. All incidental developments are incorporated into the overall landscaping of the

**A1.6** Air conditioners are not to be located in the front setback area. Air conditioning noise levels shall be in accordance with the *Environmental Protection Act* and associated noise regulations.

**A2.1** Solar collectors shall not be located on the primary street facade.

**A2.2** Application for solar collector approval requires the following:

- i. Site plan showing roof form and proposed location of solar collectors;
- ii. Type/Brand of solar unit;
- iii. Extent of solar collectors;
- iv. Details of frame support;
- v. Current drawings/photos of existing building and immediate locality; and,
- vi. Heritage status of existing building (refer MHI).

**A3** Incidental developments must not be within the front setback zone unless:

- i. The location of incidental development is not practicable behind the building line;
- ii. The height, scale and colour of the structure is compatible with the building;
- iii. The structure does not limit sight lines between premises and the road reserve; and,
- iv. The structure does not dominate the original building as viewed from the street.

**A4** Incidental developments are to be erected on the back facade and not project above the ridge line.

**A5** Relevant drawings illustrating the proposed incidental developments to be provided. This is to include a site plan, plan(s) and elevation(s) as required.

development and are screened from view from adjoining properties and streets.

**P6** All incidental elements are to be designed and constructed in accordance with the provisions of the Residential Design Codes.

**P7** Incidental developments should not display any form of advertising if visible from street level.

**A6** Relevant information illustrating performance against relevant R-Code design elements to be provided.

**A7** Discrete product brand names are acceptable.

### 3.7.14 Footpaths and Crossovers

#### 3.7.14.1 Statement

*The streetscapes in the Policy Area predominantly feature single houses on single lots with a single crossover. Crossovers are predominantly asphalt bitumen. Footpaths are evident in streetscapes and are generally of concrete or laterite finish.*



#### 3.7.14.2 Desired Outcomes

- i. New footpaths and crossovers to match existing streetscapes;
- ii. Maintenance of existing footpaths and crossovers;
- iii. Maximum of one crossover per lot or subdivided lot; and,
- iv. Street trees to be conserved or replaced where a new crossover requires their removal.

#### 3.7.14.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Pedestrian walk ways will take priority over vehicular access. Re-kerbing is to be done wherever footpaths are replaced.</p>	<p><b>A1</b> Crossovers to stop at footpaths and preserve footpath continuity and pedestrian priority.</p>

**P2** Footpaths and crossovers to match the existing relevant Precincts.

**P3** All crossovers, ramps and footpaths to the requirement of Australian Standards.

**P4** No street trees will be removed for a crossover unless approved by the Council and an approved replacement tree is planted.

**P5** Installation of crossovers and removal of redundant crossovers to be carried out after consultation with the owner of the property. Redundant crossovers to be removed, at the applicants cost, prior to the issue of a building permit for the relevant property.

**A2.1** Footpaths to be between 1.2m and 2m wide. Footpaths are to have a clearly defined edge.

**A2.2** The following outlines the prescribed maximum crossover widths:

- For lots 12 metres in width or less the maximum width is 3.0 metres; and
- For lots 12 metres or more the maximum width is 30% of the lot frontage up to a maximum width of 5.0 metres. No crossovers are to be constructed over the maximum width of 5.0 metres.

**A2.3** All crossover materials will be at the discretion of the Chief Executive Officer. Crossovers are to be constructed as per Council's Crossover Specification requirements.

**A3** All crossovers, ramps and footpaths shall be to Australian Standards.

**A4** Retain existing trees or replace existing tree where required.

**A5.1** Maximum of one crossover per lot or subdivided lot unless approved by the council.

**A5.2** Relevant drawings indicating location of existing and proposed crossover where required. This is to include existing and proposed site plans.

## Costs

- i. Minor maintenance of approved bitumen or concrete crossovers will be undertaken at the Town's expense. Where liability for damage is established, repairs may be undertaken by the Town at the relevant property owner's expense;
- ii. Maintenance of non-standard or non-approved crossovers will be at the property owner's expense;
- iii. Property owners to pay a fee representing the cost of constructing a new crossover or full replacement of an existing crossover. This fee is to be included in Council's annual scale of fees and charges; and,
- iv. Council may, in the future, introduce an application fee and annual inspection fee for brick paved crossovers.

**NOTE:** This section does not apply to Canning Highway, which is subject to separate requirements.



### 3.7.15 Woodside & Richmond Precincts

#### 3.7.15.1 Statement of Desired Future Character

##### 3.7.15.1.1 Woodside & Richmond

*The desired future character of Woodside and Richmond is the maintenance of its predominantly single storey Federation and Interwar period buildings, cohabiting with more recent infill buildings and additions in a leafy streetscape.*



#### 3.7.15.2 Access, Parking and Rights-of-Way

##### 3.7.15.2.1 Statement

*Residences in Woodside and Richmond have access via a single cross over. Traditional building patterns have parking located at the rear of the block. Parking to new developments sometimes occurs in the front setback area, in contrast to the traditional built pattern.*



### 3.7.15.2.2 Desired Development Outcomes

- i. Parking areas of lots and development sites shall reflect the existing streetscape where possible;
- ii. Where possible, parking is to be located to the rear of any new developments; and,
- iii. Number of crossovers is to be minimised.

### 3.7.15.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1.1</b> Access and parking for the building is to be adequately provided for within the boundaries of the lot/development site, and does not negatively impact on:</p> <ul style="list-style-type: none"> <li>i. The streetscape character and amenity; and,</li> <li>ii. The availability of on-street parking in the locality.</li> </ul> <p><b>P1.2</b> Council shall exercise its discretion to vary scheme requirements for places with heritage value where compliance would adversely impact on heritage places.</p>	<p><b>A1.1</b> Parking to be located at the rear of the block.</p> <p><b>A1.2</b> Additions and alterations to existing buildings shall address any additional parking requirements with reference to the R-Codes.</p> <p><b>A1.3</b> Parking areas of lots and development sites for grouped or multiple dwellings are accessed from a shared access way.</p>

### 3.7.15.3 Garages, Carports and Outbuildings

#### 3.7.15.3.1 Statement

*Garages, carports and outbuildings in the Woodside and Richmond Precincts are generally located at the rear or side of the original dwelling and are not dominant features of the landscape. More recent developments incorporate garages and carports into their front facades in contrast to the traditional pattern.*



#### 3.7.15.3.2 Desired Development Outcomes

- i. Where garages and carports are part of a development they must be incorporated into, and be compatible with the design of the dwelling;
- ii. Garages and carports shall not visually dominate the dwelling as viewed from the street;
- iii. Materials should not detract visually from the streetscape; and,
- iv. Carports associated with existing residences are not required to mimic or match the materials of the house.

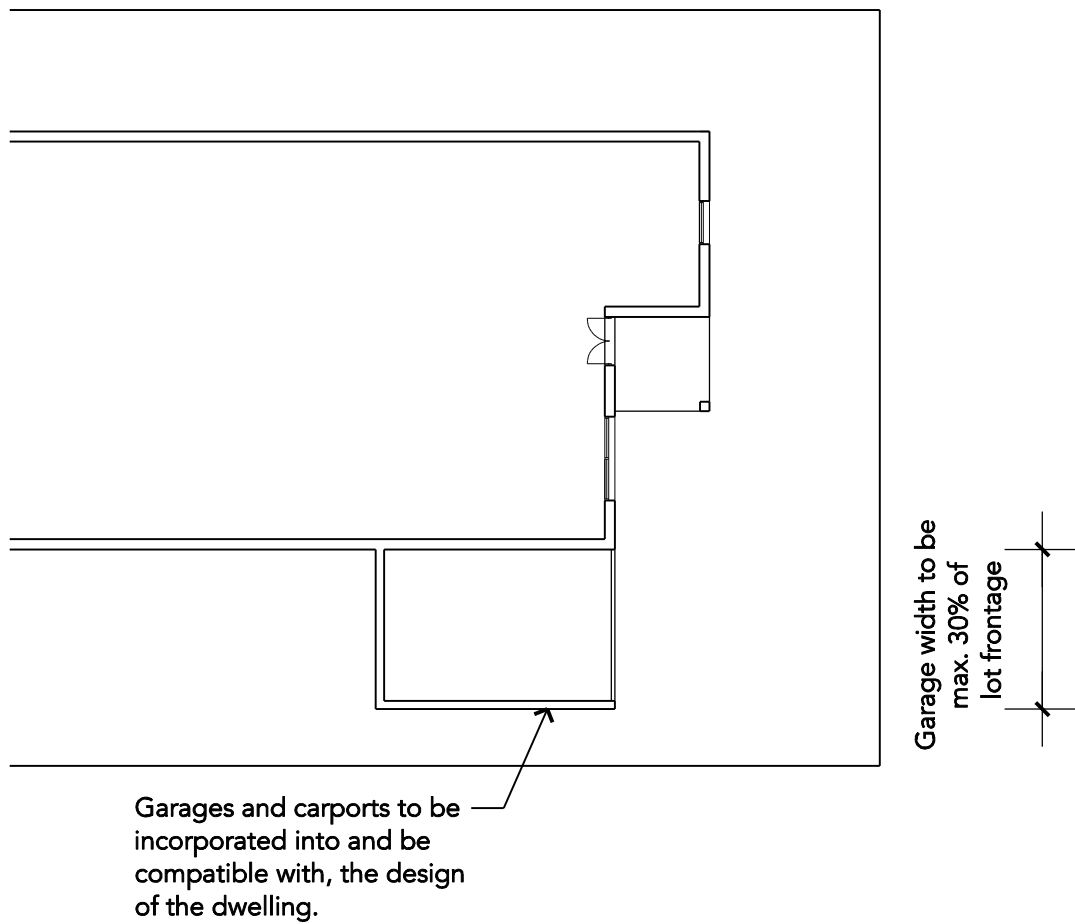


Figure 18 - Plan illustrating compatible garage in new developments - *Griffiths Architects 2012*.

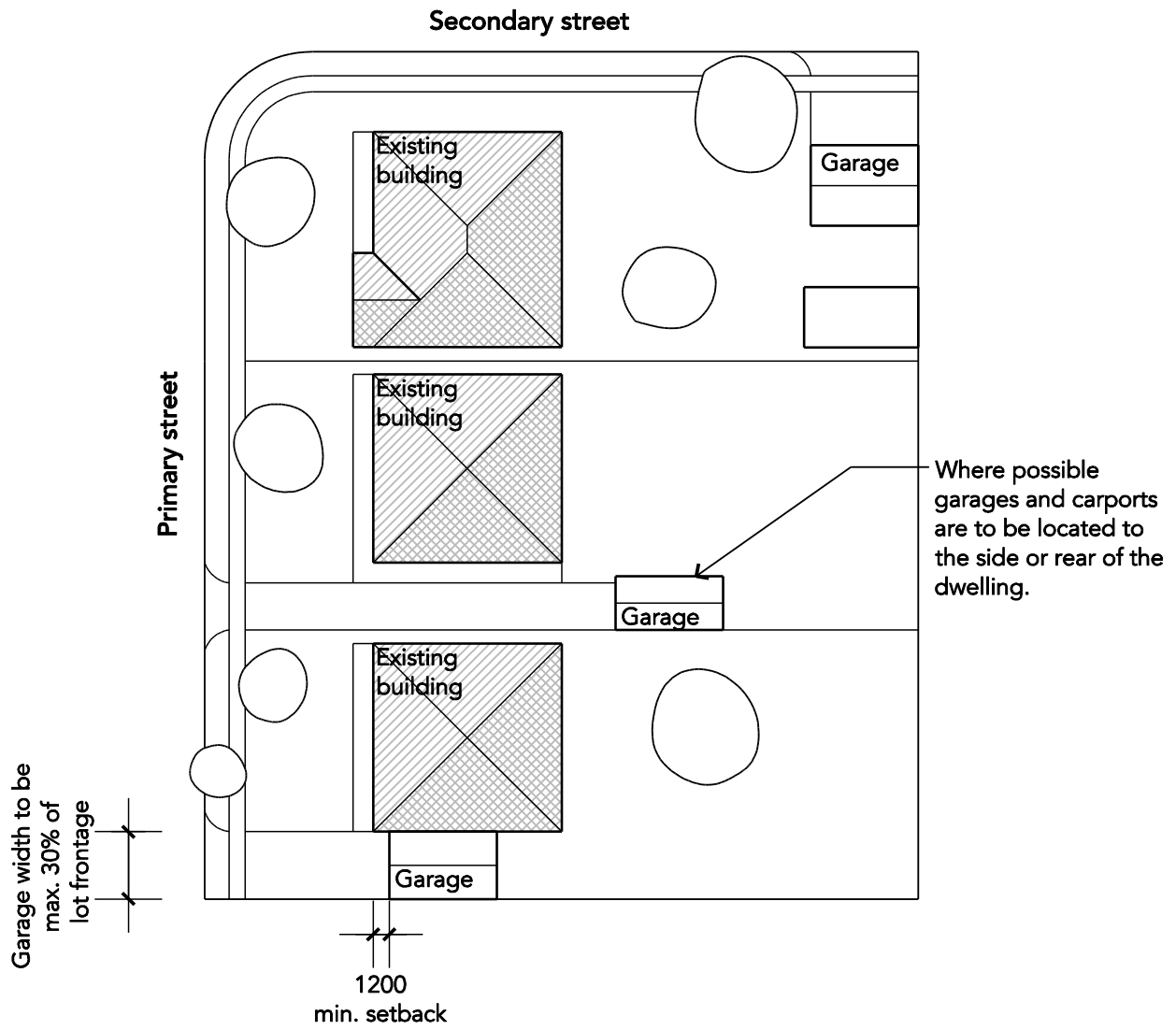


Figure 19 – Site plan illustrating compatible garages in residential developments – *Griffiths Architects 2012.*

### 3.7.15.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>All Dwellings</b></p> <p><b>P1</b> Garages, carports or outbuildings should comply with the recommended building materials for the Precinct.</p>	<p><b>All Dwellings</b></p> <p><b>A1</b> Refer to applicable Materials and Colours section for relevant precinct. Plans, elevations and section drawings are to be provided to demonstrate the impact of the garage or carport on the existing dwelling.</p>
<p><b>New Dwellings</b></p> <p><b>P2</b> Garages and carports are designed to be incorporated into, and compatible with, the design of the dwelling.</p>	<p><b>New Dwellings</b></p> <p><b>A2</b> Plans, elevations and section drawings are to be provided to demonstrate the impact of the garage or carport on the new dwelling.</p>
<p><b>Existing Dwellings</b></p> <p><b>P3</b> For existing buildings where there are no alternatives, carports may be located forward of the building line, provided they:</p> <ul style="list-style-type: none"> <li>i. Do not visually dominate the streetscape or the buildings to which they belong; and,</li> <li>ii. Do not detract from the heritage character of a contributory building. Street elevations are to be included including a minimum of the subject lot and two neighbouring lots.</li> </ul>	<p><b>Existing Dwellings</b></p> <p><b>A3</b> For existing buildings garages and carports are constructed behind the building line and comply with the following:</p> <ul style="list-style-type: none"> <li>i. Setback a minimum distance of 1.2m behind the building line; and,</li> <li>ii. The width of garages and carports are not greater than 30% of the frontage of the lot.</li> </ul>
<p><b>Multiple and Grouped Dwellings</b></p> <p><b>P4</b> Adequate car and bicycle parking provided on-site in accordance with development requirements.</p> <p><b>P5</b> Garages and carports shall not be prominent on the primary street façade.</p>	<p><b>Multiple and Grouped Dwellings</b></p> <p><b>A4</b> Refer R Codes for car space and bicycle space requirements.</p> <p><b>A5</b> Garages designed at the rear of the lot.</p>

### 3.7.15.4 Building Design Requirements

#### 3.7.15.4.1 Building Height, Form, Scale and Bulk

##### 3.7.15.4.1.1 Statement

*Dwellings that contribute positively to the character and significance of the Woodside and Richmond Precincts are generally of similar form, bulk and scale. The prevailing building typology is modest single storey residences facing the street. This is such a strong characteristic that single storey, and compatible two storey development, should be the maximum building envelope. Lot sizes and setbacks are also consistent throughout both Precincts. In any new development, the form, bulk and scale will need to be demonstrably compatible with the existing and surrounding residences.*



##### 3.7.15.4.1.2 Desired Development Outcomes

- i. New developments should reflect the prevailing form, bulk and scale of the immediate locality;
- ii. New developments shall respect and follow the predominant street pattern in terms of roof pitch, orientation and articulation; and,
- iii. The bulk of two storey developments and additions should be located towards the rear of the lot and not be visually dominant when viewed from the primary street.

##### 3.7.15.4.1.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> New developments, additions and alterations to be of a compatible form, bulk and scale to traditional development in the immediate locality.</p>	<p><b>A1.1</b> Developments to comply with all design elements of this Local Planning Policy.</p> <p><b>A1.2</b> Additions and alterations are single storey or designed so that the upper storey is not visually dominant when viewed from the primary street.</p> <p><b>A1.3</b> Two storey developments to comply with the following:</p> <ol style="list-style-type: none"> <li>i. Second storey is setback so that the roof line is below the line of sight of a person standing on the opposite side of the primary street.</li> </ol>

**A1.4** Category 'B' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable as the 'Acceptable Development' standards where:

- i. significant water views from neighbouring properties will not be affected;
- ii. the 'Acceptable Development' standards of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy are met.
- iii. the subject site is not a battle-axe lot.

**A1.5** In localities where views are an important part of the amenity of the area and neighbours existing views are to be affected, or the subject site is a 'battle-axe' lot, then the maximum building heights are as follows:

- .. 8.1m to the top of a pitched roof
- .. 6.5m to the top of an external wall (concealed roof)
- .. 5.6m to the top of an external wall; and where the following apply:

- i. The proposal demonstrates design, bulk and scale that responds to adjacent development and the established character of the area or other site specific circumstances;
- ii. The provision of a landscaping plan demonstrating a minimum of 50% of the effective lot area being landscaped; and,
- iii. Subject to the 'Acceptable Development' standards of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy being met.

**A1.6** Category 'A' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable for development which does not meet the requirements of A1.4 and A1.5 above.



### 3.7.15.4.2 Verandahs and Porches

#### 3.7.15.4.2.1 Statement

*Verandahs and porches are a strong contributor to the character of the Woodside and Richmond Precincts and should be encouraged in new developments. This feature is an essential unifying element that contributes to the streetscape character of both Precincts.*



#### 3.7.15.4.2.2 Desired Development Outcomes

- i. Verandahs and porches to be encouraged in new developments. They should complement the immediate locality; and,
- ii. Existing verandahs and porches should be conserved, and missing verandahs reinstated.

#### 3.7.15.4.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1.1</b> The principle façade and main entrance of a new building should address the street in a manner that reflects the predominant character of the traditional development in the immediate locality.</p> <p><b>P1.2</b> Contributory buildings retain their primary access and conserve the traditional detailing of verandahs and porches.</p> <p><b>P1.3</b> Missing verandahs to be reinstated.</p>	<p><b>A1.1</b> New developments to include a verandah or porch that addresses the primary street and comprises the primary access to the building.</p> <p><b>A1.2</b> Additions and alterations to a contributory building to retain or reinstate the existing verandah or porch. Where appropriate and information available restore/reinstate their original detailing.</p>

### 3.7.15.4.3 Fremantle Port Buffer

#### 3.7.15.4.3.1 Statement

*Fremantle Ports undertook the Fremantle Inner Harbour Definition Study (May 2002), which identified the need for an offsite buffer around the Port. The buffer was determined on a range of potential amenity impacts and risks including noise, odour and public risk.*

Three buffer areas around the Port have been identified; Area 1, Area 2 and Area 3. For the purposes of these Guidelines these areas will be known as Buffer Area 1, Buffer Area 2 and Buffer Area 3. The buffer area relevant to the Woodside and Richmond Precincts is Buffer Area 3 (refer figures 20 and 21).

Generally the potential risk and amenity impacts from the Port are considerably less in Buffer Area 3. Nevertheless, the Fremantle Inner Harbour Buffer Definition Study has identified the potential for some noise and odour impacts in this area.

The intent of Buffer Area 3 is the management, as opposed to the control, of sensitive uses.

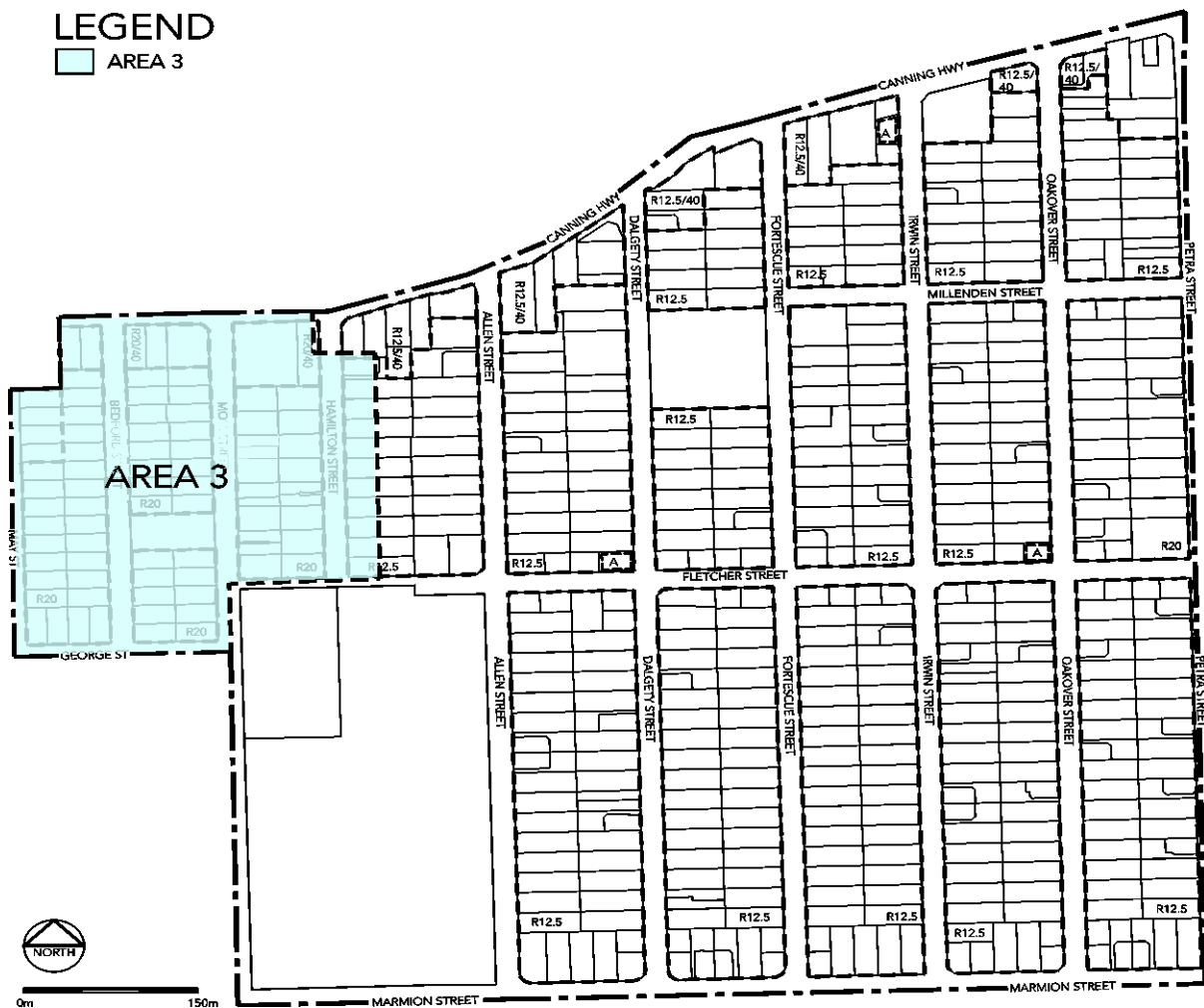


Figure 20- Fremantle Port Buffer Zone Area 3 affecting the Woodside Precinct. Griffiths Architects 2011

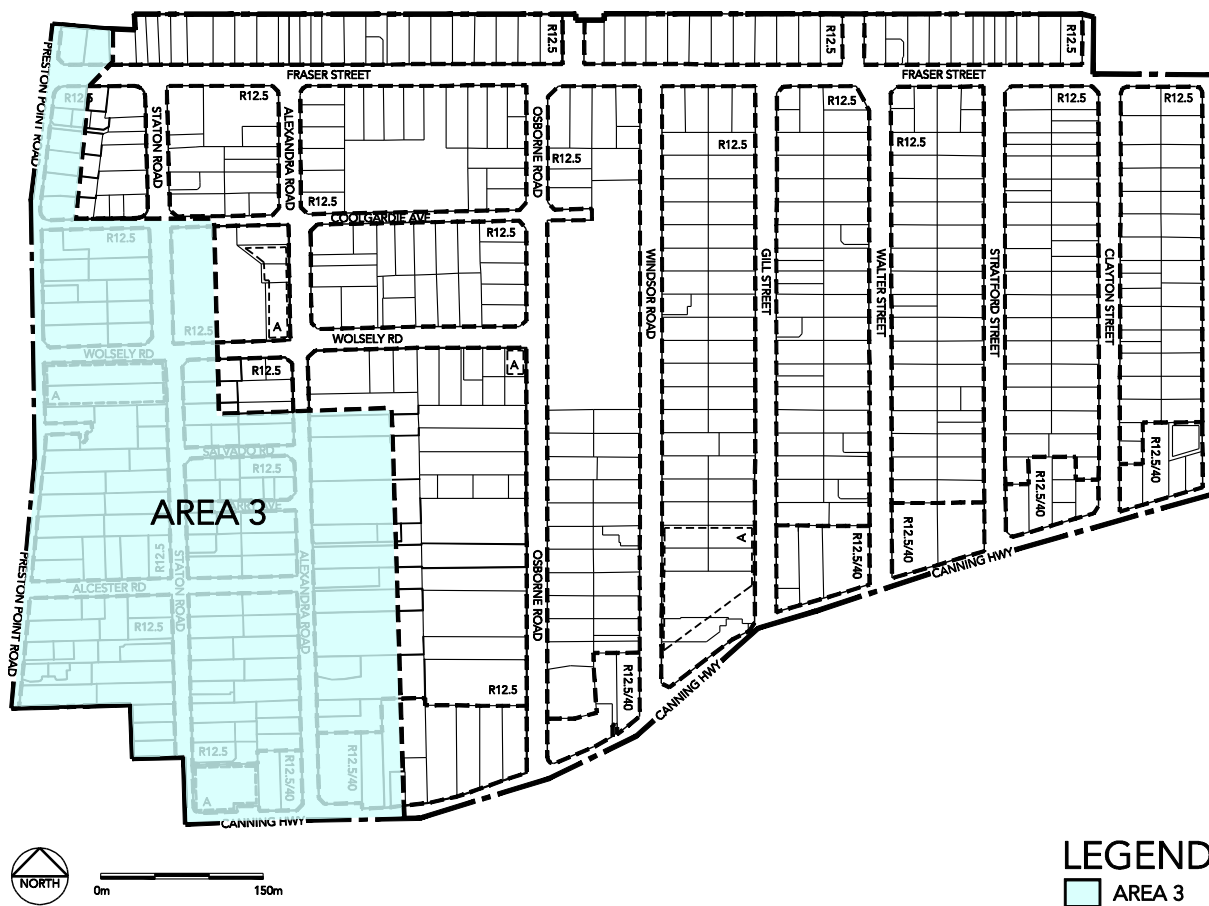


Figure 21- Fremantle Port Buffer Zone Area 3 affecting the Richmond Precinct. *Griffiths Architects 2011*

### 3.7.15.4.3.2 Desired Development Outcomes

- i. To minimise potential impacts that may arise from the Port;
- ii. To define separate land use and built form requirements for each buffer area; and,
- iii. To manage sensitive uses.

### 3.7.15.4.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> There are no general buffer-related development controls for Buffer Area 3. However, where a specific location within this area is known to be impacted from port operations (e.g. through a history of formal complaints), Council may, in consultation with Fremantle Ports, apply some or all of the development controls of Buffer Area 2.</p>	<p><b>A1.1</b> Developments, additions and alterations must comply with the Fremantle Buffer Area requirements.</p> <p><b>A1.2</b> Council may accept alternative built form treatments subject to the applicant satisfactorily demonstrating fulfilment of the potential risk and amenity considerations outlined above. Alternative treatments shall be justified to</p>

Council through submission of professionally prepared and certified reports.

#### **3.7.15.4.3.4 Administrative Procedures**

Where applicable, applicants should be advised as soon as possible of the requirements of this policy. Ideally, this should be prior to lodging a formal application for development, including proposals for subdivision and scheme amendments.

Applicants should be encouraged to liaise with relevant staff including those at Fremantle Ports, in order to understand the requirements of this policy.

Council shall refer a proposal to Fremantle Ports where a proponent seeks any significant variation to the development controls contained within these guidelines.

Fremantle Port shall within 14 days of notification, advise the Town of its assessment of a development proposal referred as per the requirements outlined above.

In terms of conditions of development approval that arise from the requirements of this policy, Council shall require a building surveyor or suitably qualified structural engineer to certify that the requirements of the conditions have been fulfilled in accordance with the approved plans.

Where appropriate, certification shall be provided prior to the issue of a building permit, certificate of clearance / classification or strata / subdivision clearance.

The applicant shall arrange for certification to be endorsed by Fremantle Ports prior to lodgement of appropriate documentation with the Town.

#### **Buffer Area 3**

Council shall refer a proposal to Fremantle Ports where the proposal falls within a specific location that has been formally notified to Council as being impacted from port operations.

In the case of scheme amendments that would result in an increase or intensification of sensitive uses, Council shall notify Fremantle Ports as soon as practicable prior to initiating the amendment.

### 3.7.16 Plympton Precinct

#### 3.7.16.1 Statement of Desired Future Character

##### 3.7.16.1.1 Plympton

*The desired future character of Plympton is the maintenance of its predominant single storey gold boom period buildings, cohabiting with sympathetic infill and additions in a leafy streetscape. Similarly the George Street Precinct should retain its heritage and seek to encourage new development or redevelopment that makes a complementary and positive contribution to the streetscape.*



#### 3.7.16.2 Access, Parking and Rights-of-Way

##### 3.7.16.2.1 Statement

*Residences in Plympton have access via a single cross over. Traditional building patterns have parking located on street. Parking to new developments sometimes occurs in the front setback area, in contrast to the traditional built pattern.*



### 3.7.16.2.2 Desired Development Outcomes

- i. Parking areas of lots and development sites shall reflect the existing streetscape where possible;
- ii. Where possible, parking is to be located to the side of any new developments; and,
- iii. Number of crossovers is to be minimised.

### 3.7.16.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1.1</b> Access and parking for the building is to be adequately provided for within the boundaries of the lot/development site, and does not negatively impact on:</p> <ul style="list-style-type: none"> <li>i. The streetscape character and amenity; and,</li> <li>ii. The availability of on-street parking in the locality.</li> </ul> <p><b>P1.2</b> Council shall exercise its discretion to vary scheme requirements for places with heritage value where compliance would adversely impact on heritage places.</p>	<p><b>A1.1</b> Parking to be located to the side of the dwelling.</p> <p><b>A1.2</b> Additions and alterations to existing buildings shall address any additional parking requirements with reference to the R-Codes.</p> <p><b>A1.3</b> Parking areas of lots and development sites for grouped or multiple dwellings are accessed from a shared access way.</p>

### 3.7.16.3 Garages, Carports and Outbuildings

#### 3.7.16.3.1 Statement

*Garages, carports and outbuildings are a rare occurrence in the Plympton Precincts. Where garages and carports do occur they are associated with more recent developments. Garages and carports are incorporated into the design of the dwelling and are not dominant features of the streetscape.*

*Garages and carports are discouraged in the Plympton Precinct.*



### 3.7.16.3.2 Desired Development Outcomes

- i. Absence of garages and carports in the streetscape;
- ii. Where garages and carports are part of a development they must be incorporated and be compatible with the design of the dwelling;
- iii. Garages and carports shall not visually dominate the dwelling as viewed from the street;
- iv. Materials should not detract visually from the streetscape; and,
- v. Carports associated with existing residences are not required to mimic or match the materials of the house.

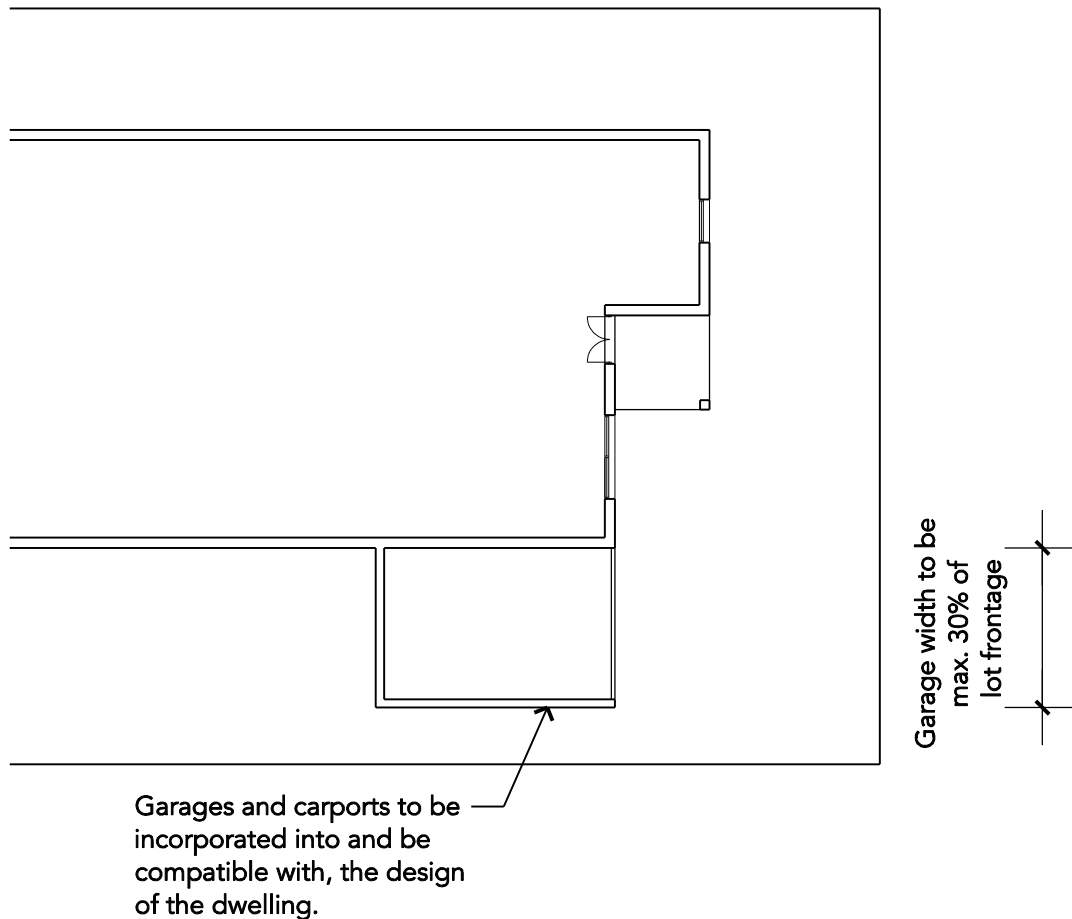


Figure 22 – Plan illustrating compatible garage in new developments – *Griffiths Architects 2012*.

### 3.7.16.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>All Dwellings</b></p> <p><b>P1</b> Garage and carports are to not adversely impact the streetscape.</p> <p><b>P2</b> Garages, carports or outbuildings should comply with the recommended building materials for the Precinct.</p>	<p><b>All Dwellings</b></p> <p><b>A1</b> On street parking.</p> <p><b>A2</b> Refer Section Materials and Colours. Plans, elevations and section drawings are to be provided to demonstrate</p>

### New Dwellings

- P3** Where garages and carports are required they are to be designed to be incorporated into, and compatible with, the design of the dwelling.

### Multiple and Grouped Dwellings

- P4** Adequate car and bicycle parking provided on-site in accordance with development requirements.
- P5** Garages and carports shall not be prominent on the primary street façade. Street elevations may be required where a place is included in the MHL.

the impact of the garage or carport on the existing dwelling.

### New Dwellings

- A3** Plans, elevations and section drawings are to be provided to demonstrate the impact of the garage or carport on the new dwelling.

### Multiple and Grouped Dwellings

- A4** Refer R Codes for car space and bicycle space requirements.
- A5** Garages designed at the rear of the lot.

## 3.7.16.4 Building Design Requirements

### 3.7.16.4.1 Building Height, Form, Scale & Bulk

#### 3.7.16.4.1.1 Statement

*Dwellings that contribute positively to the character and significance of the Plympton Precinct are generally of similar form, bulk and scale. The prevailing building typology is narrow single storey residences facing the street. This is such a strong characteristic that single storey, and compatible two storey development, should be the maximum building envelope (refer glossary). Two storey developments are evident in the Plympton Precinct. Lot sizes and setbacks are also consistent throughout both Precincts. In any new development, the form, bulk and scale will need to be demonstrably compatible with the existing and surrounding residences.*





### 3.7.16.4.1.2 Desired Development Outcomes

- i. New developments should reflect the prevailing form, bulk and scale of the immediate locality;
- ii. New developments shall respect and follow the predominant street pattern in terms of roof pitch, orientation and articulation; and,
- iii. Two storey developments and additions that are well designed and do not visually dominant the immediate locality.

### 3.7.16.4.1.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> New developments, additions and alterations to be of a compatible form, bulk and scale to traditional development in the immediate locality.</p>	<p><b>A1.1</b> Developments to comply with all design elements of this Local Planning Policy.</p> <p><b>A1.2</b> Additions and alterations to a contributory dwelling are single storey and leave the existing building completely intact.</p> <p><b>A1.3</b> The bulk of two storey developments to be located at the rear half of the lot.</p> <p><b>A1.4</b> Category 'A' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable as the 'Acceptable Development' standards unless otherwise varied by application under A1.5.</p> <p><b>A1.5</b> Category 'B' will apply as set out within Table 3 – Maximum Building Heights of the Residential Design Codes.</p> <ul style="list-style-type: none"> <li>i. The proposal demonstrates design, bulk and scale that responds to neighbouring developments and the established character of the existing development on the site or other site specific circumstances;</li> <li>ii. Subject to compliance with the 'Acceptable Development' standards of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy.</li> </ul>

- iii. The proposed development does not unreasonably impact upon the amenity of the principal outdoor living area of adjacent properties.
- iv. The subject lot is not a battle axe lot.

NOTE: Refer to the Town Centre Redevelopment Guidelines for lots adjacent to the Canning Highway.

### 3.7.16.4.2 Verandahs and Porches

#### 3.7.16.4.2.1 Statement

*Verandahs and porches are a strong contributor to the character of the Plympton Precinct and should be encouraged in new developments. This feature is an essential unifying element that contributes to the streetscape character of both Precincts.*



#### 3.7.16.4.2.2 Desired Development Outcomes

- i. Verandahs and porches are to be encouraged in new developments. They should complement the immediate locality; and,
- ii. Existing verandahs and porches should be conserved, and missing verandahs reinstated.

#### 3.7.16.4.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> The principle façade and main entrance of a new building should address the street in a manner that reflects the predominant character of the traditional development in the immediate locality.</p>	<p><b>A1</b> New developments to include a verandah or porch that address the primary street and comprises the primary access to the building.</p>

**P2** Contributory buildings retain their primary access and conserve the traditional detailing of verandahs and porches.

**P3** Missing verandahs to be reinstated.

**A2** Additions and alterations to a contributory building to retain or reinstate the existing verandah or porch. Where appropriate and information available restore/reinstate their original detailing.

**A3** Reinstate missing verandahs.

### 3.7.16.4.3 Fremantle Port Buffer

#### 3.7.16.4.3.1 Statement

*Fremantle Ports undertook the Fremantle Inner Harbour Definition Study (May 2002), which identified the need for an offsite buffer around the Port. The buffer was determined on a range of potential amenity impacts and risks including noise, odour and public risk.*

*Three buffer areas around the Port have been identified; Area 1, Area 2 and Area 3. For the purposes of these Guidelines these areas will be known as Buffer Area 1, Buffer Area 2 and Buffer Area 3. The buffer areas relevant to the Plympton Precinct are Buffer Area 2 and 3 (refer figure 23).*

*For buildings within Buffer Area 2 consideration is given to the following potential impacts:*

- i. Ingress of toxic gases in the event of an incident within the Port;*
- ii. Shattering or flying glass as a consequence of explosion within the Port;*
- iii. Noise transmission emanating from the Port (attenuation in the order of 30dB(A) is required; and,*
- iv. Odour.*

*Generally the potential risk and amenity impacts from the Port are considerably less in Buffer Area 3. Nevertheless, the Fremantle Inner Harbour Buffer Definition Study has identified the potential for some noise and odour impacts in this area.*

*The intent of Buffer Area 3 is the management, as opposed to the control, of sensitive uses.*



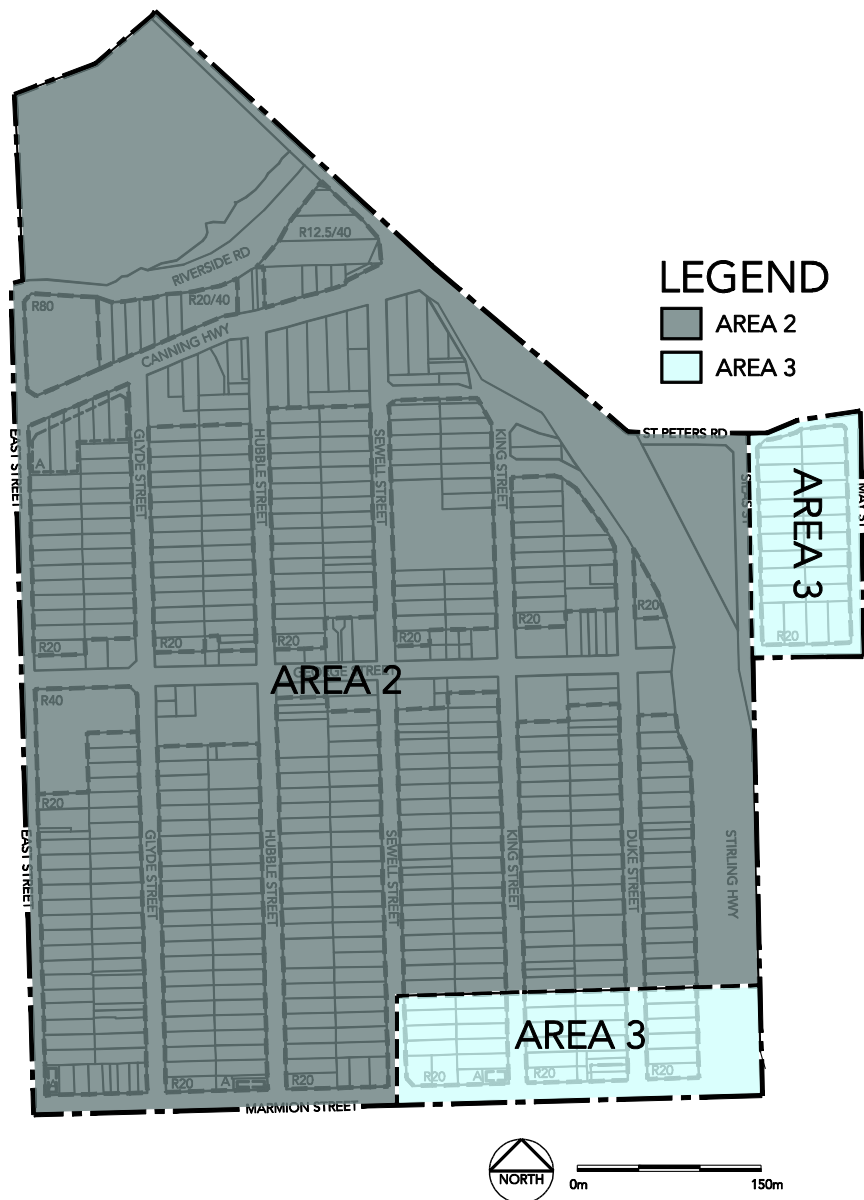


Figure 23- Fremantle Port Buffer Zone Area 2 and Area 3 affecting the Plympton Precinct. *Griffiths Architects 2011*

### 3.7.16.4.3.2 Desired Development Outcomes

- i. To minimise potential impacts that may arise from the Port;
- ii. To define separate land use and built form requirements for each buffer area; and,
- iii. To manage sensitive uses.

### 3.7.16.4.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Buffer Area 2 built form requirements shall apply to all residential development other than alterations and additions to existing dwellings.</p>	<p><b>A1</b> Buffer Area 2 built form requirements</p> <ul style="list-style-type: none"> <li>i. Windows and opening requirements:</li> <li>ii. Any glass used for windows or other openings shall be laminated safety glass of minimum thickness of 6mm or “double glazed” utilising laminated or toughened safety glass of a minimum thickness of 3mm; and,</li> <li>iii. All safety glass shall be manufactured and installed to an appropriate Australian Standard.</li> <li>iv. Air Conditioning Requirements: <ul style="list-style-type: none"> <li>(a) Multiple systems to have internally centrally located shut down point and associated procedures for emergency use; and,</li> <li>(b) Preference for split “refrigerative” systems.</li> </ul> </li> <li>v. Construction requirements: <ul style="list-style-type: none"> <li>(a) Adopt the general principles of quiet house design for residential developments; and,</li> <li>(b) All developments shall incorporate roof insulation.</li> </ul> </li> </ul>
<p><b>P2</b> There are no general buffer-related development controls for Buffer Area 3. However, where a specific location within this area is known to be impacted from port operations (eg through a history of formal complaints), Council may, in consultation with Fremantle Ports, apply some or all of the development controls of Buffer Area 2.</p>	<p><b>A2.1</b> Developments, additions and alterations must comply with the Fremantle Buffer Area requirements.</p> <p><b>A2.2</b> Council may accept alternative built form treatments subject to the applicant satisfactorily demonstrating fulfilment of the potential risk and amenity considerations outlined above. Alternative treatments shall be justified to Council through submission of professionally prepared and certified reports.</p>

**NOTE:** Council recognises that these requirements may not be possible to achieve in the case of the proposals involving some buildings of conservation and heritage significance.

All residential development approvals shall be conditioned in order to require a notification to be placed on title advising of the potential amenity impacts associated with living / working in proximity of the Port.

In the case of all residential subdivision, Council and Fremantle Ports shall request the Western Australian Planning Commission to support the placing of memorials on new titles advising of the potential amenity impacts associated with living in proximity of the Port.

#### **3.7.16.4.3.4 Administrative Procedures**

Where applicable, applicants should be advised as soon as possible of the requirements of this policy. Ideally, this should be prior to lodging a formal application for development, including proposals for subdivision and scheme amendments.

Applicants should be encouraged to liaise with relevant staff including those at Fremantle Ports, in order to understand the requirements of this policy.

Council shall refer a proposal to Fremantle Ports where a proponent seeks any significant variation to the development controls contained within these guidelines.

Fremantle Port shall within 14 days of notification, advise the Town of its assessment of a development proposal referred as per the requirements outlined above.

In terms of conditions of development approval that arise from the requirements of this policy, Council shall require a building surveyor or suitably qualified structural engineer to certify that the requirements of the conditions have been fulfilled in accordance with the approved plans.

Where appropriate, certification shall be provided prior to the issue of a building permit, certificate of clearance / classification or strata / subdivision clearance.

The applicant shall arrange for certification to be endorsed by Fremantle Ports prior to lodgement of appropriate documentation with the Town.

#### **Buffer Area 3**

Council shall refer a proposal to Fremantle Ports where the proposal falls within a specific location that has been formally notified to Council as being impacted from port operations.

In the case of scheme amendments that would result in an increase or intensification of sensitive uses, Council shall notify Fremantle Ports as soon as practicable prior to initiating the amendment.

### 3.7.17 Richmond Hill Precinct

#### 3.7.17.1 Statement of Desired Future Character

##### 3.7.17.1.1 Richmond Hill

*The desired future character of Richmond Hill is the maintenance of its traditional buildings, cohabiting with developments that are river orientated, a sympathetic scale and are of contemporary architectural style.*



#### 3.7.17.2 Access, Parking and Rights-of-Way

##### 3.7.17.2.1 Statement

*Residences in the Richmond Hill Precinct have access predominantly via a single cross over. Multiple dwellings occur in the western area of Richmond Hill. Parking to multiple dwellings occurs at the rear via a shared access. Some multiple dwellings have multiple crossovers. Traditional building patterns have parking located at the rear of the block. Parking to new single developments often occurs in the front setback area, in contrast to the traditional built pattern.*



##### 3.7.17.2.2 Desired Development Outcomes

- i. Parking areas of lots and development sites shall reflect the existing streetscape where possible;
- ii. Where possible, parking to multiple dwelling to occur at the rear or side of the lot; and,
- iii. Number of crossovers is to be minimized.

### 3.7.17.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Access and parking for the building is to be adequately provided for within the boundaries of the lot/development site, and does not negatively impact on:</p>	<p><b>A1.1</b> Parking to occur at the rear of the lot.</p>
<p>i. The streetscape character and amenity; and,</p>	<p><b>A1.2</b> Additions and alterations to existing buildings shall address any additional parking requirements with reference to the R-Codes.</p>
<p>ii. The availability of on-street parking in the locality.</p>	<p><b>A1.3</b> Parking areas of lots and development sites for grouped or multiple dwellings are accessed from a shared access way.</p>
<p><b>P2</b> Garages and carports shall not be prominent on the primary street façade. Street elevations may be required where a place is included in the MHL.</p>	<p><b>A2</b> Garages designed at the rear of the lot.</p>

### 3.7.17.3 Garages, Carports and Outbuildings

#### 3.7.17.3.1 Statement

*Garages, carports and outbuildings in the Richmond Hill Precinct are generally incorporated into the front facade. Parking is located to the rear and sides of the traditional buildings.*

*Garages and carports are compatible with the Richmond Hill Precinct.*



#### 3.7.17.3.2 Desired Development Outcomes

- i. Garages and carports shall be incorporated into and be compatible with, the design of the dwelling;
- ii. Garages and carports shall not visually dominate the dwelling as viewed from the street;
- iii. Where possible garages and carports to multiple dwellings to occur at the rear and side of the lot;
- iv. Materials should not detract visually from the streetscape; and,
- v. Carports associated with existing residences are not required to mimic or match the materials of the house.



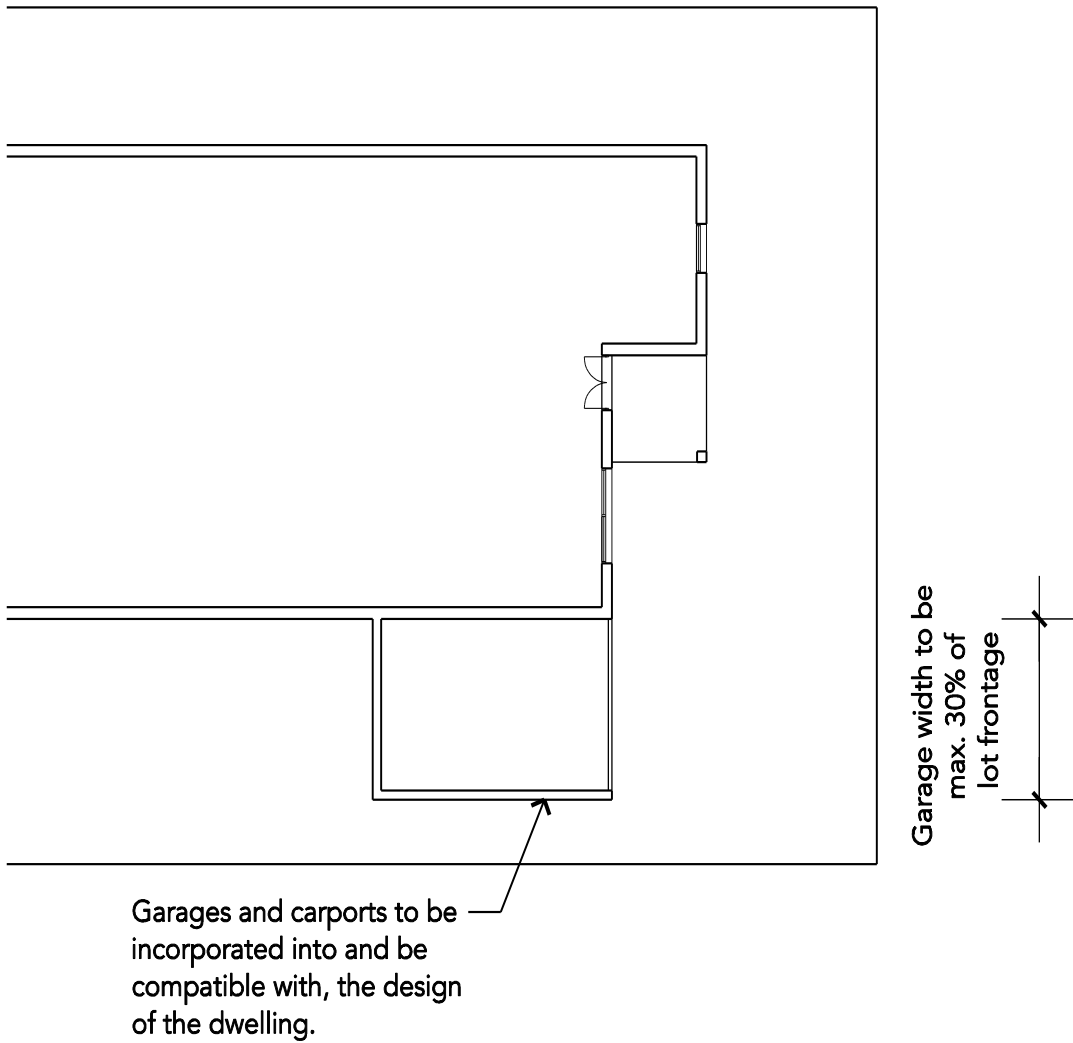


Figure 24 - Plan illustrating compatible garage in new developments - *Griffiths Architects 2012.*

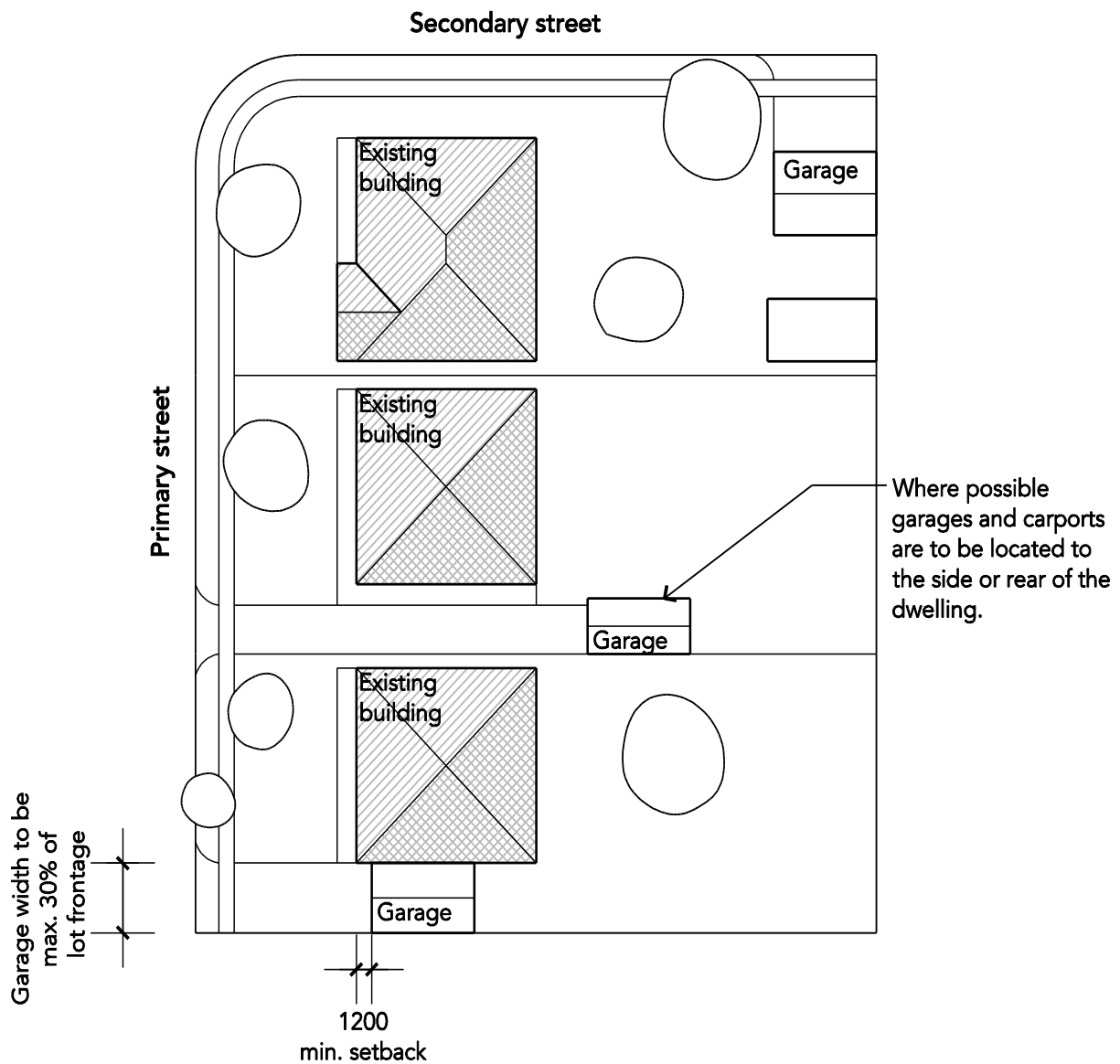


Figure 25 - Site plan illustrating compatible garage locations in residential developments - Griffiths Architects 2012.

### 3.7.17.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>All Dwellings</b></p> <p><b>P1</b> Garages to new developments do not visually detract from the streetscape.</p> <p><b>P2</b> Garages, carports or outbuildings should comply with the recommended building materials for the Precinct.</p>	<p><b>All Dwellings</b></p> <p><b>A1</b> Garages are incorporated into the front façade.</p> <p><b>A2</b> Refer Section Materials and Colours. Plans, elevations and section drawings are to be provided to demonstrate the impact of the garage or carport on the existing dwelling.</p>

### Existing Dwellings

- P3** For existing contributory buildings where there are no alternatives, carports may be located forward of the building line, provided they:
- i. Do not visually dominate the streetscape or the buildings to which they belong; and,
  - ii. Do not detract from the heritage character of a contributory building.

### New Dwellings

- P4** Garages and carports are designed to be incorporated into, and compatible with, the design of the dwelling.

### Multiple and Grouped Dwellings

- P5** Adequate car and bicycle parking provided on-site in accordance with development requirements.
- P6** Garages and carports shall not be prominent on the primary street façade.

### Existing Dwellings

- A3** Garages and carports are constructed behind the building line and comply with the following:
- i. Setback a minimum distance of 1.2m behind the building line; and,
  - ii. The width of garages and carports are not greater than 30% of the frontage of the lot.

### New Dwellings

- A4** Plans, elevations and section drawings are to be provided to demonstrate the impact of the garage or carport on the new dwelling.

### Multiple and Grouped Dwellings

- A5** Refer R Codes for car space and bicycle space requirements.
- A6** Garages designed at the rear of the lot.

## 3.7.17.4 Building Design Requirements

### 3.7.17.4.1 Building Height, Form, Scale & Bulk

#### 3.7.17.4.1.1 Statement

*Dwellings that contribute positively to the character and significance of the Richmond Hill Precinct are generally of similar form, bulk and scale. The prevailing form is substantial single and double storey residences orientated towards the river. Residences with river views are such a strong characteristic that the scale of new developments should reflect the scale of the immediate locality. Single and two storey dwellings are compatible in the Richmond Hill Precinct. Lot sizes and setbacks are also consistent throughout the Precinct. In any new development, the form, bulk and scale will need to be demonstrably compatible with the existing and surrounding residences.*



### 3.7.17.4.1.2 Desired Development Outcomes

- i. New developments should reflect the prevailing form, bulk and scale of the immediate locality; and,
- ii. New developments shall respect and follow the predominant street pattern in terms of roof pitch, orientation and articulation.

### 3.7.17.4.1.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> New developments, additions and alterations to be of a compatible form, bulk and scale to traditional development in the immediate locality.</p>	<p><b>A1.1</b> Developments to comply with all design elements of this Local Planning Policy.</p> <p><b>A1.2</b> Additions and alterations are single storey and located at the rear of the existing dwelling. The existing building remains intact.</p> <p><b>A1.3</b> Category 'B' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable as the 'Acceptable Development' standards where:</p> <ul style="list-style-type: none"> <li>i. significant water views from neighbouring properties will not be affected</li> <li>ii. the 'Acceptable Development' standards of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy are met.</li> <li>iii. the subject site is not a battle axe lot.</li> </ul> <p><b>A1.4</b> In localities where views are an important part of the amenity of the area and neighbours existing views are</p>

to be affected, or the subject site is a 'battle axe' lot, then the maximum building heights are as follows:

- .. 8.1m to the top of a pitched roof
- .. 6.5m to the top of an external wall (concealed roof)
- .. 5.6m to the top of an external wall; and where the following apply.

- i. The proposal demonstrates design, bulk and scale that responds to adjacent development and the established character of the area or other site specific circumstances;
- ii. The provision of a landscaping plan demonstrating a minimum of 50% of the effective lot area being landscaped; and,
- iii. Subject to the 'Acceptable Development' standards of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy being met.

**A1.5** Category 'A' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable for development which does not meet the requirements of A1.3 and A1.4 above.

### 3.7.17.4.2 Verandahs and Porches

#### 3.7.17.4.2.1 Statement

*Verandahs generally occur in the Federation building forms. Porches are present in the Post World War II era buildings. These should be conserved and missing examples reinstated. Although verandahs and porches are evident in the Richmond Hill Precinct they are not a strong contributor to the overall streetscape character. However, verandahs and porches are a preferred façade feature.*



### 3.7.17.4.2.2 Desired Development Outcomes

- i. Verandahs and porches to be preferred in new developments. Verandahs and porches to reflect architectural period of construction without negatively impacting on the immediate locality; and,
- ii. Existing verandahs and porches should be conserved, and missing verandahs reinstated.

### 3.7.17.4.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> The principle façade and main entrance of a new building should address the street in a manner that reflects the predominant character of the traditional development in the immediate locality.</p>	<p><b>A1</b> New developments to include a verandah or porch that address the primary street and comprises the primary access to the building.</p>
<p><b>P2</b> Where verandahs or porches are included in new developments they are to reflect the architectural period of construction and do not adversely affect the immediate locality.</p>	<p><b>A2</b> Relevant drawings to demonstrate impact on immediate locality to be provided. This is to include plans, elevations and possible sections.</p>
<p><b>P3.1</b> Contributory buildings retain their primary access and conserve the traditional detailing of verandahs and porches.</p>	<p><b>A3</b> Additions and alterations to a contributory building to retain or reinstate the existing verandah or porch. Where appropriate and information available restore/reinstate their original detailing.</p>
<p><b>P3.2</b> Missing verandahs to be reinstated.</p>	

### 3.7.17.4.3 Fremantle Port Buffer

#### 3.7.17.4.3.1 Statement

*Fremantle Ports undertook the Fremantle Inner Harbour Definition Study (May 2002), which identified the need for an offsite buffer around the Port. The buffer was determined on a range of potential amenity impacts and risks including noise, odour and public risk.*

*Three buffer areas around the Port have been identified; Area 1, Area 2 and Area 3. For the purposes of these Guidelines these areas will be known as Buffer Area 1, Buffer Area 2 and Buffer Area 3. The buffer area relevant to the Richmond Hill Precinct is Buffer Area 3 (refer figure 26).*

*Generally the potential risk and amenity impacts from the Port are considerably less in Buffer Area 3. Nevertheless, the Fremantle Inner Harbour Buffer Definition Study has identified the potential for some noise and odour impacts in this area.*

*The intent of Buffer Area 3 is the management, as opposed to the control, of sensitive uses.*



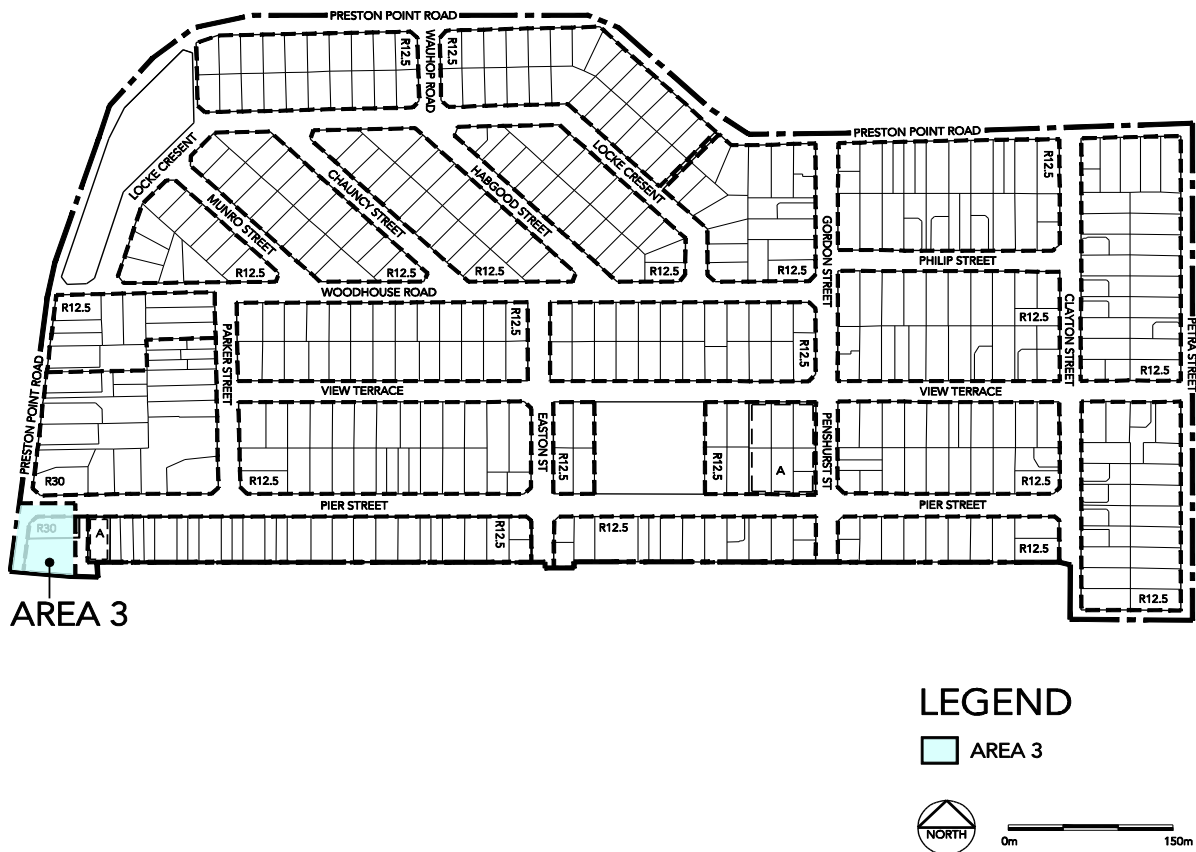


Figure 26- Fremantle Port Buffer Zone Area 3 affecting the Richmond Hill Precinct. *Griffiths Architects 2011*

### 3.7.17.4.3.2 Desired Development Outcomes

- i. To minimise potential impacts that may arise from the Port;
- ii. To define separate land use and built form requirements for each buffer area; and,
- iii. To manage sensitive uses.

### 3.7.17.4.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> There are no general buffer related development controls for Buffer Area 3. However, where a specific location within this area is known to be impacted from port operations (e.g. through a history of formal complaints), Council may, in consultation with Fremantle Ports, apply some or all of the development controls of Buffer Area 2.</p>	<p><b>A1.1</b> Developments, additions and alterations must comply with the Fremantle Buffer Area requirements.</p> <p><b>A1.2</b> Council may accept alternative built form treatments subject to the applicant satisfactorily demonstrating fulfilment of the potential risk and amenity considerations outlined above. Alternative treatments shall be justified to Council through submission of professionally prepared and certified reports.</p>



#### **3.7.17.4.3.4 Administrative Procedures**

Where applicable, applicants should be advised as soon as possible of the requirements of this policy. Ideally, this should be prior to lodging a formal application for development, including proposals for subdivision and scheme amendments.

Applicants should be encouraged to liaise with relevant staff including those at Fremantle Ports, in order to understand the requirements of this policy.

Council shall refer a proposal to Fremantle Ports where a proponent seeks any significant variation to the development controls contained within these guidelines.

Fremantle Port shall within 14 days of notification, advise the Town of its assessment of a development proposal referred as per the requirements outlined above.

In terms of conditions of development approval that arise from the requirements of this policy, Council shall require a building surveyor or suitably qualified structural engineer to certify that the requirements of the conditions have been fulfilled in accordance with the approved plans.

Where appropriate, certification shall be provided prior to the issue of a building permit, certificate of clearance / classification or strata / subdivision clearance.

The applicant shall arrange for certification to be endorsed by Fremantle Ports prior to lodgement of appropriate documentation with the Town.

#### **Buffer Area 3**

Council shall refer a proposal to Fremantle Ports where the proposal falls within a specific location that has been formally notified to Council as being impacted from port operations.

In the case of scheme amendments that would result in an increase or intensification of sensitive uses, Council shall notify Fremantle Ports as soon as practicable prior to initiating the amendment.

### 3.7.18 Riverside

#### 3.7.18.1 Statement of Desired Future Character

##### 3.7.18.1.1 Riverside

*The desired future character of Riverside is the maintenance of its limited number of single storey Federation period buildings, cohabiting with sympathetic, appropriate scale infill and additions in narrow streetscapes.*



#### 3.7.18.2 Access, Parking and Rights-of-Way

##### 3.7.18.2.1 Statement

##### 3.7.18.2.1.1 Riverside South

*Riverside South is bounded by Stirling Highway (north inclusive), Riverside Road (east inclusive), Preston Point Road (west inclusive), Bolton Street (south inclusive) and Canning Highway (north inclusive).*

*Residences in Riverside South have access via a single cross over. Traditional building patterns have parking located at the front of the block. Parking to new developments sometimes occurs in the rear setback area, in contrast to the traditional built pattern.*



### 3.7.18.2.1.2 Riverside North

*Riverside North is bounded by Riverside Road (east inclusive), Preston Point Road (west inclusive) and Bolton Street (north inclusive).*

*Residences in Riverside North have access via a single shared cross over. Parking to multiple dwellings occurs at the front of lots and within the front setback area.*

### 3.7.18.2.2 Desired Development Outcomes

- i. Parking areas of lots and development sites shall reflect the existing streetscape where possible;
- ii. Where possible, parking is to be located to the side of any new developments; and,
- iii. Number of crossovers is to be minimised.

### 3.7.18.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Access and parking for the building is to be adequately provided for within the boundaries of the lot/development site, and does not negatively impact on:</p> <ul style="list-style-type: none"> <li>i. The streetscape character and amenity; and,</li> <li>ii. The availability of on-street parking in the locality.</li> </ul>	<p><b>A1.1</b> Parking areas associated with developments shall reflect the existing streetscapes.</p> <p><b>A1.2</b> Additions and alterations to existing buildings shall address any additional parking requirements with reference to the R-Codes.</p> <p><b>A1.3</b> Parking areas of lots and development sites for grouped or multiple dwellings are accessed from a shared access way.</p>
<p><b>P2.1</b> Where possible parking to multiple dwellings to occur on the lot where appropriate to allow access to river views but still presenting an agreeable streetscape elevation.</p>	<p><b>A2</b> Parking to multiple dwelling is designed so that dwellings have maximum access to views. However the design of multiple dwellings should not negate the streetscape in favour of views.</p>
<p><b>P2.2</b> Council shall exercise its discretion to vary scheme requirements for places with heritage value where compliance would adversely impact on heritage places.</p>	

### 3.7.18.3 Garages, Carports and Outbuildings

#### 3.7.18.3.1 Statement

*The location of garages, carports and outbuildings in the Riverside Precinct are mixed i.e. they are not predominantly located to the front rear or side. In some locations they are dominant features of the landscape.*

*Garages and carports are compatible in the Riverside Precinct.*



### 3.7.18.3.2 Desired Development Outcomes

- i. Where garages and carports are part of a development they must be incorporated into, and be compatible with the design of the dwelling;
- ii. Garages and carports shall not visually dominate the dwelling as viewed from the street;
- iii. Where possible garages and carports to multiple dwellings shall not visually dominate the dwelling as viewed from the street;
- iv. Materials should not detract visually from the streetscape; and,
- v. Carports associated with existing residences are not required to mimic or match the materials of the house.

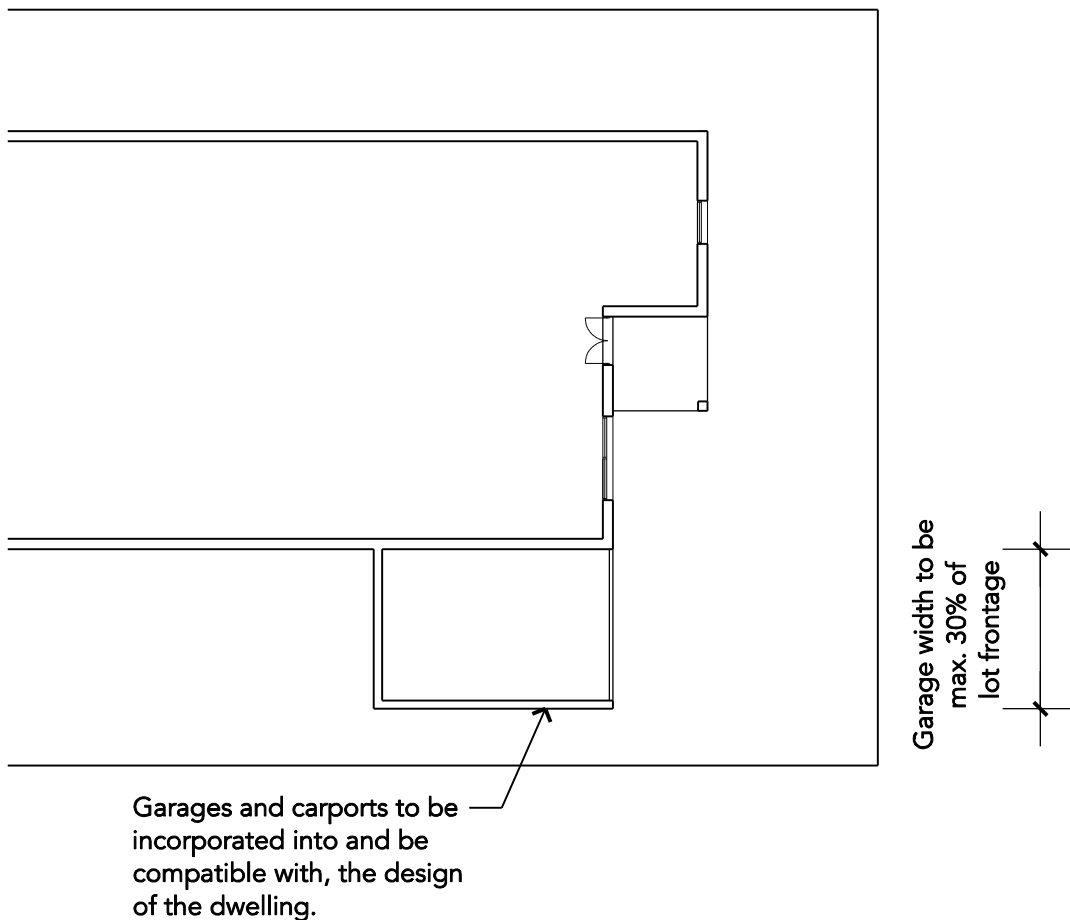


Figure 27 – Plan illustrating compatible garage in new developments – *Griffiths Architects 2012*.

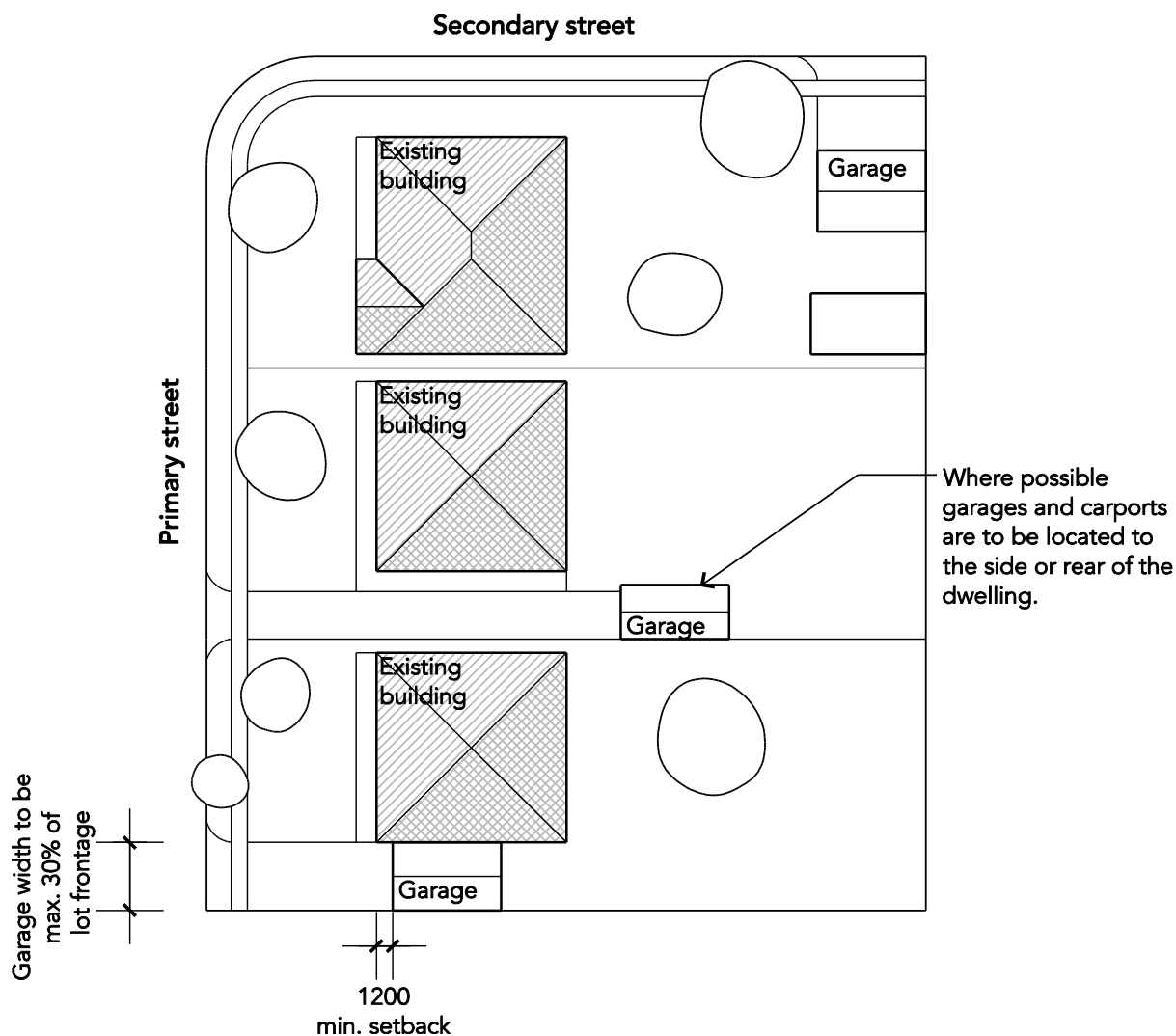


Figure 28 - Site plan illustrating compatible garage locations in residential developments - *Griffiths Architects 2012*.

### 3.7.18.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>All Dwellings</b></p> <p><b>P1</b> Garages, carports or outbuildings should comply with the recommended building materials for the Precinct.</p> <p><b>P2</b> For existing buildings where there are no alternatives, carports may be located forward of the building line, provided they:</p>	<p><b>All Dwellings</b></p> <p><b>A1</b> Refer Section on Materials and Colours. Plans, elevations and section drawings are to be provided to demonstrate the impact of the garage or carport on the existing dwelling.</p> <p><b>A2</b> Garages and carports are constructed behind the building line and comply with the following:</p> <ul style="list-style-type: none"> <li>i. Setback a minimum distance of 1.2m behind the building line; and,</li> </ul>

- i. Do not visually dominate the streetscape or the buildings to which they belong; and,
- ii. Do not detract from the heritage character of a contributory building. Street elevations may be required where a place is included in the MHI.

**New Dwellings**

**P3** Garages and carports are designed to be incorporated into, and compatible with, the design of the dwelling.

**Multiple and Grouped Dwellings**

- P4** Adequate car and bicycle parking provided on-site in accordance with development requirements.
- P5** Where possible garages and carports to multiple dwellings to occur at the rear and side of the lot.

- ii. The width of garages and carports are not greater than 30% of the frontage of the lot.

**New Dwellings**

**A3** Plans, elevations and section drawings are to be provided to demonstrate the impact of the garage on the new dwelling.

**Multiple and Grouped Dwellings**

- A4** Refer R Codes for car space and bicycle space requirements.
- A5** Garages to multiple dwelling are designed so that dwellings have maximum access to views. However the design of multiple dwellings should not negate the streetscape in favour of views.

**3.7.18.4 Building Design Requirements**

**3.7.18.4.1 Building Height, Form, Scale & Bulk**

**3.7.18.4.1.1 Statement**

**3.7.18.4.1.1.1 Riverside South**

*The prevailing built form is large multiple storey residences built along the crest of a hill. Developments display a variety of construction methods with varying scale and bulk. Lot sizes and setbacks vary depending on the topography of the area. Building bulk and orientation is predominantly defined by river views.*



### 3.7.18.4.1.1.2 Riverside North

*The prevailing building typology is double storey masonry construction with tiles and corrugated iron roofing. The prevailing building stock is from the last 30 years.*



### 3.7.18.4.1.2 Desired Development Outcomes

- i. Appropriate design approach to the topography of the landscape;
- ii. New developments should reflect the prevailing form, bulk and scale of the immediate locality;
- iii. New developments shall respect and follow the immediate locality in terms of roof pitch, orientation and articulation; and,
- iv. The bulk of multiple storey developments and additions should be appropriate to the topography of the landscape.

### 3.7.18.4.1.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> New developments, additions and alterations to be of a compatible form, bulk and scale to traditional development in the immediate locality.</p>	<p><b>A1.1</b> Developments to comply with all design elements of this Local Planning Policy.</p> <p><b>A1.2</b> Additions and alterations are single storey and located at the rear of the existing dwelling. The existing building remains intact.</p>
<p><b>P2</b> Form and bulk of new developments to be designed appropriately to the topography of the landscape.</p>	<p><b>A2.1</b> Relevant drawings to demonstrate impact on the existing topography to be provided. This is to include site plans, plans, elevations and sections.</p> <p><b>A2.2</b> Two storey developments comply with the following:</p> <ol style="list-style-type: none"> <li>i. Reflect the immediate locality in terms of setbacks and form; and,</li> <li>ii. They do not visually dominate the streetscape.</li> </ol>

**A2.3** Category 'B' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable as the 'Acceptable Development' standards where:

- i. significant water views from neighbouring properties will not be affected;
- ii. the 'Acceptable Development' standards of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy are met; and
- iii. the subject site is not a battle axe lot.

**A2.4** In localities where views are an important part of the amenity of the area and neighbours existing views are to be affected, or the subject site is a 'battle-axe' lot, then the maximum building heights are as follows:

- .. 8.1m to the top of a pitched roof
- .. 6.5m to the top of an external wall (concealed roof)
- .. 5.6m to the top of an external wall; and where the following apply.

- i. The proposal demonstrates design, bulk and scale that responds to adjacent development and the established character of the area or other site specific circumstances;
- ii. The provision of a landscaping plan demonstrating a minimum of 50% of the effective lot area being landscaped; and,
- iii. Subject to the 'Acceptable Development' standards of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy being met.

**A2.5** Category 'A' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable for development which does not meet the requirements of A2.3 and A2.4 above.



### 3.7.18.4.2 Verandahs and Porches

#### 3.7.18.4.2.1 Statement

##### 3.7.18.4.2.1.1 Riverside South

*Verandahs and porches are a strong contributor to the character of the Riverside South and should be encouraged in new developments. This feature is an essential unifying element that contributes to the streetscape character of both Precincts.*



##### 3.7.18.4.2.1.2 Riverside North

*Verandahs and porches do not contribute to the character of Riverside North and are not required in new developments.*

#### 3.7.18.4.2.2 Desired Development Outcomes

- i. Verandahs and porches to be encouraged in new developments in Riverside South. They should complement the immediate locality; and,
- ii. Existing verandahs and porches should be conserved, and missing verandahs reinstated.

#### 3.7.18.4.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>Riverside South</b></p> <p><b>P1.1</b> The principle façade and main entrance of a new building should address the street in a manner that reflects the predominant character of the traditional development in the immediate locality.</p> <p><b>P1.2</b> Contributory buildings retain their primary access and conserve the traditional detailing of verandahs and porches.</p>	<p><b>Riverside South</b></p> <p><b>A1.1</b> New developments to include a verandah or porch that address the primary street and comprises the primary access to the building.</p> <p><b>A1.2</b> Additions and alterations to a contributory building to retain or reinstate the existing verandah or porch. Where appropriate and information available restore/reinstate their original detailing.</p>

**P1.3** Missing verandahs to be reinstated.

### Riverside North

**P2** The principle façade and main entrance of a new building should address the street in a manner that reflects the predominant character of the traditional development in the immediate locality.

### Riverside North

**A2** New developments to have openings that are prominent to the street.

## 3.7.18.4.3 Fremantle Port Buffer

### 3.7.18.4.3.1 Statement

*Fremantle Ports undertook the Fremantle Inner Harbour Definition Study (May 2002), which identified the need for an offsite buffer around the Port. The buffer was determined on a range of potential amenity impacts and risks including noise, odour and public risk.*

*Three buffer areas around the Port have been identified; Area 1, Area 2 and Area 3. For the purposes of these Guidelines these areas will be known as Buffer Area 1, Buffer Area 2 and Buffer Area 3. The buffer area relevant to the Riverside Precinct is Buffer Area 2 and 3 (refer figure 29).*

*For buildings within Buffer Area 2 consideration is given to the following potential impacts:*

- *Ingress of toxic gases in the event of an incident within the Port;*
- *Shattering or flying glass as a consequence of explosion within the Port;*
- *Noise transmission emanating from the Port (attenuation in the order of 30dB(A) is required; and,*
- *Odour.*

*Generally the potential risk and amenity impacts from the Port are considerably less in Buffer Area 3. Nevertheless, the Fremantle Inner Harbour Buffer Definition Study has identified the potential for some noise and odour impacts in this area.*

*The intent of Buffer Area 3 is the management, as opposed to the control, of sensitive uses.*



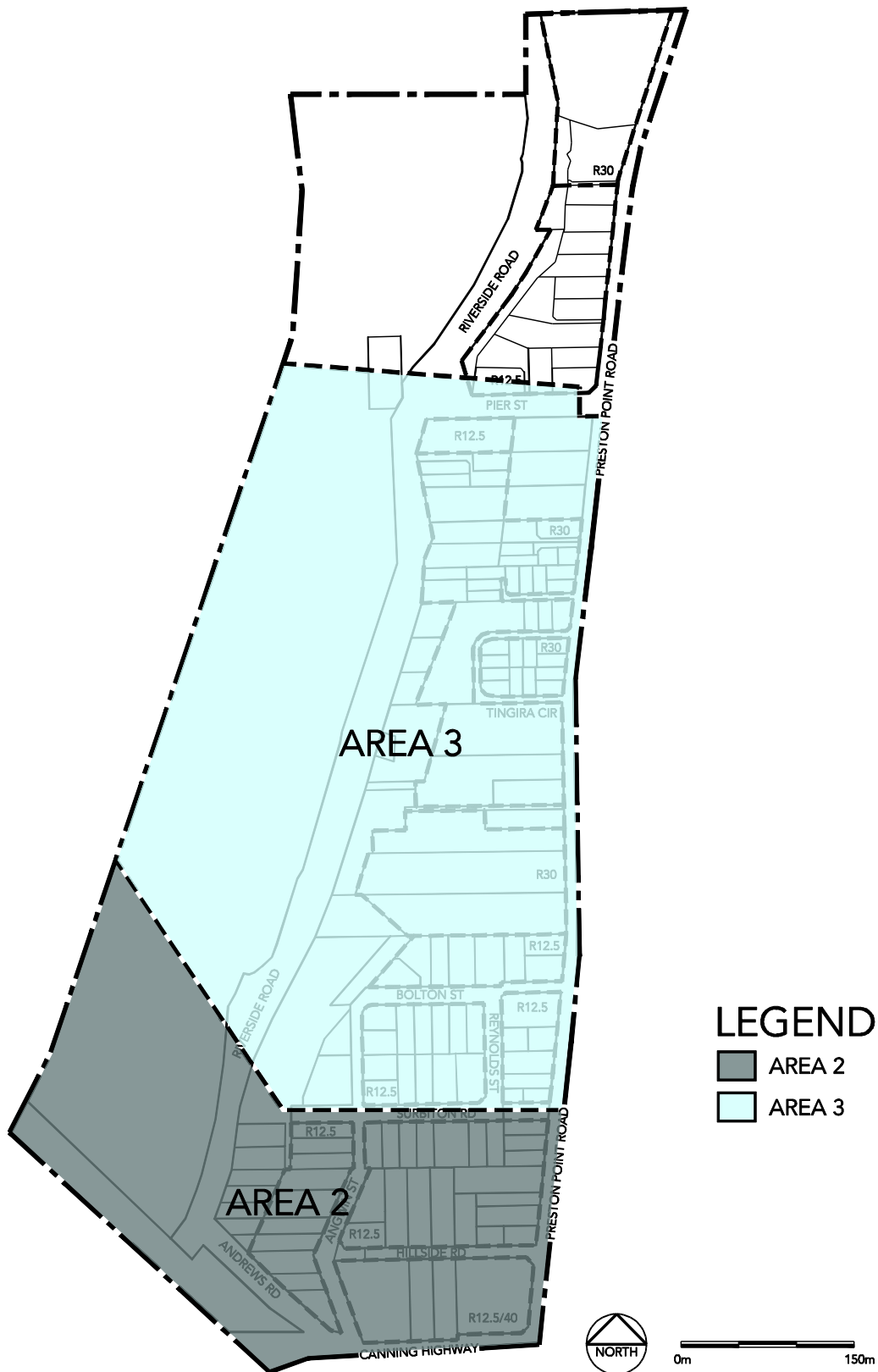


Figure 29- Fremantle Port Buffer Zone Areas 2 & 3 affecting the Riverside Precinct. *Griffiths Architects 2011*

### 3.7.18.4.3.2 Desired Development Outcomes

- i. To minimise potential impacts that may arise from the Port;
- ii. To define separate land use and built form requirements for each buffer area; and,
- iii. To manage sensitive uses.

### 3.7.18.4.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Buffer Area 2 built form requirements shall apply to all residential development other than alterations and additions to existing dwellings.</p>	<p><b>A1</b> Buffer Area 2 built form requirements</p> <ul style="list-style-type: none"> <li>i. Windows and opening requirements:               <ul style="list-style-type: none"> <li>a. Any glass used for windows or other openings shall be laminated safety glass of minimum thickness of 6mm or “double glazed” utilising laminated or toughened safety glass of a minimum thickness of 3mm, and</li> <li>b. All safety glass shall be manufactured and installed to an appropriate Australian Standard.</li> </ul> </li> <li>ii. Air Conditioning Requirements:               <ul style="list-style-type: none"> <li>a. Multiple systems to have internally centrally located shut down point and associated procedures for emergency use, and</li> <li>b. Preference for split “refrigerative” systems.</li> </ul> </li> <li>iii. Construction requirements:               <ul style="list-style-type: none"> <li>a. Adopt the general principles of quiet house design for residential developments, and</li> <li>b. All developments shall incorporate roof insulation.</li> </ul> </li> </ul>
<p><b>P2</b> There are no general buffer-related development controls for Buffer Area 3. However, where a specific location within this area is known to be impacted from port operations (e.g. through a history of formal complaints), Council may, in consultation with Fremantle Ports, apply some or all of the development controls of Buffer Area 2.</p>	<p><b>A2.1</b> Developments, additions and alterations must comply with the Fremantle Buffer Area requirements.</p> <p><b>A2.2</b> Council may accept alternative built form treatments subject to the applicant satisfactorily demonstrating fulfilment of the potential risk and amenity considerations outlined above. Alternative treatments shall be justified to</p>

Council through submission of professionally prepared and certified reports.

**NOTE:** Council recognises that these requirements may not be possible to achieve in the case of the proposals involving some buildings of conservation and heritage significance.

All residential development approvals shall be conditioned in order to require a notification to be placed on title advising of the potential amenity impacts associated with living / working in proximity of the Port.

In the case of all residential subdivision, Council and Fremantle Ports shall request the Western Australian Planning Commission to support the placing of memorials on new titles advising of the potential amenity impacts associated with living in proximity of the Port.

#### **3.7.18.4.3.4 Administrative Procedures**

Where applicable, applicants should be advised as soon as possible of the requirements of this policy. Ideally, this should be prior to lodging a formal application for development, including proposals for subdivision and scheme amendments.

Applicants should be encouraged to liaise with relevant staff including those at Fremantle Ports, in order to understand the requirements of this policy.

Council shall refer a proposal to Fremantle Ports where a proponent seeks any significant variation to the development controls contained within these guidelines.

Fremantle Port shall within 14 days of notification, advise the Town of its assessment of a development proposal referred as per the requirements outlined above.

In terms of conditions of development approval that arise from the requirements of this policy, Council shall require a building surveyor or suitably qualified structural engineer to certify that the requirements of the conditions have been fulfilled in accordance with the approved plans.

Where appropriate, certification shall be provided prior to the issue of a building permit, certificate of clearance / classification or strata / subdivision clearance.

The applicant shall arrange for certification to be endorsed by Fremantle Ports prior to lodgement of appropriate documentation with the Town.

#### **Buffer Area 3**

Council shall refer a proposal to Fremantle Ports where the proposal falls within a specific location that has been formally notified to Council as being impacted from port operations.

In the case of scheme amendments that would result in an increase or intensification of sensitive uses, Council shall notify Fremantle Ports as soon as practicable prior to initiating the amendment.

### 3.7.19 Preston Point Precinct

*The Preston Point Precinct contains no residential development. The guidelines below offer a proposed set of desired outcomes for residential development in the event that Leeuwin Barracks is decommissioned. The guidelines are based upon the existing built fabric and topography.*

*Before any residential development occurs the following assessments and plans should be made:*

- i. Heritage assessment of the site and its existing buildings;*
- ii. A structure plan; and,*
- iii. A detailed area plan.*



#### 3.7.19.1 Statement of Desired Future Character

##### 3.7.19.1.1 Preston Point

*The desired future character of the Preston Point precinct is the maintenance of its commercial and recreational buildings along the foreshore, cohabiting with green public open space. Should Leeuwin Barracks be decommissioned, the place should be conserved, adapted and developed in accordance with ICOMOS Burra Charter principles. Any new residential buildings should contribute to the values of the significant Leeuwin Barrack Buildings.*

Under the State Governments "Directions 2031 and Beyond" growth strategy for metropolitan Perth the Town is required to seek the addition of 600 hundred dwellings within its remit. Consideration should be given to the suitability of a substantial element of medium density housing in this Precinct to meet part of this requirement.

#### 3.7.19.2 Development Pattern and Lot Subdivision

##### 3.7.19.2.1 Statement

*The first objective of the structure plan would be to establish densities, circulation and a development pattern that's compatible with the Heritage values of the site and to propose a pattern of subdivisions for the site.*

### 3.7.20 Richmond Raceway Precinct

#### 3.7.20.1 Statement of Desired Future Character

##### 3.7.20.1.1 Richmond Raceway

*The desired future character of the Richmond Raceway Precinct is to maintain the medium density development and to ensure that residences retain their association with the streetscape.*



#### 3.7.20.2 Access, Parking and Rights-of-Way

##### 3.7.20.2.1 Statement

*Residences in the Richmond Raceway Precinct have access predominantly via a single cross over. Parking to the majority of developments occurs in the front setback area. There are a couple of multiple dwellings in the Richmond Raceway Precinct. Parking to multiple dwellings occurs at the rear of the lots is via shared access over multiple crossovers.*



##### 3.7.20.2.2 Desired Development Outcomes

- i. Parking areas of lots and development sites shall reflect the existing streetscape where possible;
- ii. Where possible parking to multiple dwelling to occur at the rear or side of the lot. Access is to be from a shared access way; and,
- iii. Minimise number of crossovers.

### 3.7.20.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> Access and parking for the building is to be adequately provided for within the boundaries of the lot/development site, and does not negatively impact on:</p> <ul style="list-style-type: none"> <li>i. The streetscape character and amenity; and,</li> <li>ii. The availability of on-street parking in the locality.</li> </ul> <p><b>P2</b> Parking for all dwelling types shall not be prominent on the primary street facade.</p>	<p><b>A1</b> Parking areas of lots and development sites for grouped or multiple dwellings are accessed from a shared access way.</p> <p><b>A2</b> Parking to all dwellings to be designed at the rear of the lot.</p>

### 3.7.20.3 Garages, Carports and Outbuildings

#### 3.7.20.3.1 Statement

*Garages, carports and outbuildings in the Richmond Raceway Precinct are generally incorporated into the front facade. Parking is to be located to the rear of dwellings.*

*Garages and carports are compatible with the Richmond Raceway Precinct.*



#### 3.7.20.3.2 Desired Development Outcomes

- i. Garages and carports shall be incorporated into and be compatible with, the design of the dwelling;
- ii. Garages and carports shall not visually dominate the dwelling as viewed from the street;
- iii. Materials should not detract visually from the streetscape; and,
- iv. Carports associated with existing residences are not required to mimic or match the materials of the house.



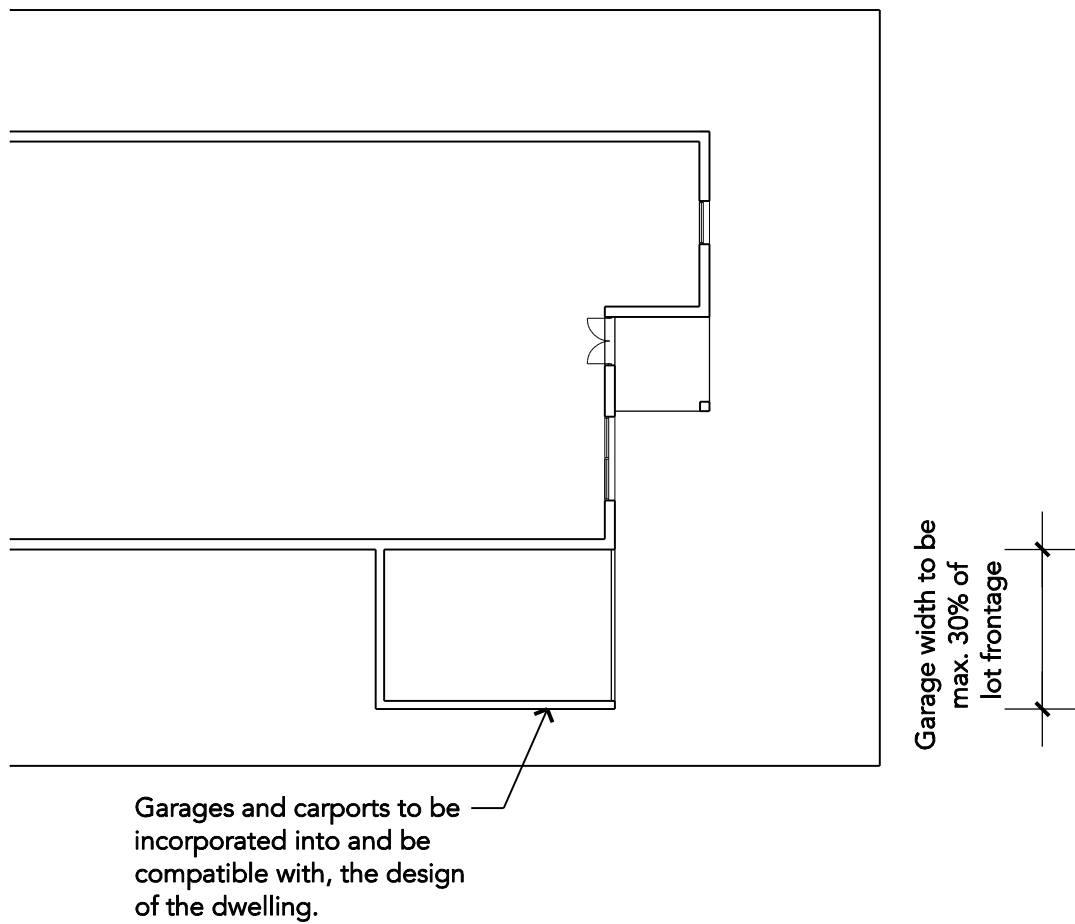


Figure 30 - Plan illustrating compatible garage in new developments - *Griffiths Architects 2012.*

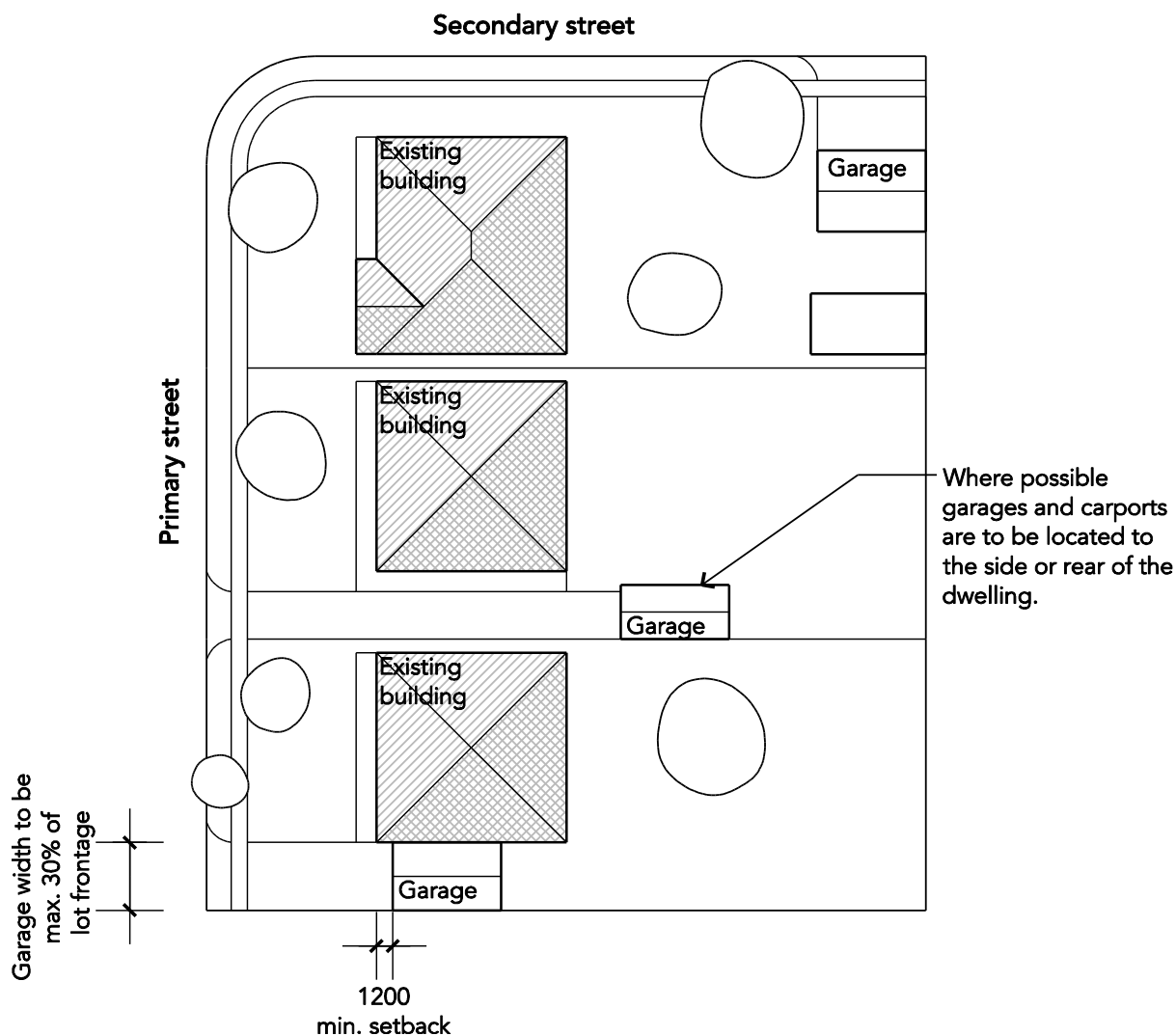


Figure 31 – Site plan illustrating compatible garage locations in residential developments – *Griffiths Architects 2012*.

### 3.7.20.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>All Dwellings</b></p> <p><b>P1</b> Garages, carports or outbuildings should comply with the recommended building materials for the Precinct.</p>	<p><b>All Dwellings</b></p> <p><b>A1</b> Refer Section on Materials and Colours.</p>
<p><b>Existing Dwellings</b></p> <p><b>P2</b> For existing buildings where there are no alternatives, carports may be located forward of the building line, provided they:</p>	<p><b>Existing Dwellings</b></p> <p><b>A2</b> Garages and carports are constructed behind the building line and comply with the following:</p> <ul style="list-style-type: none"> <li>i. Setback a minimum distance of 1.2m behind the building line; and,</li> </ul>

- i. Do not visually dominate the streetscape or the buildings to which they belong; and.

**New Dwellings**

**P3** Garages and carports are designed to be incorporated into, and compatible with, the design of the dwelling.

**Multiple and Grouped Dwellings**

**P4.1** Adequate car and bicycle parking provided on-site in accordance with development requirements.

**P4.2** Garages and carports shall not be prominent on the primary street facade.

- ii. The width of garages and carports are not greater than 30% of the frontage of the lot.

**New Dwellings**

**A3** Plans, elevations and section drawings are to be provided.

**Multiple and Grouped Dwellings**

**A4.1** Refer R Codes for car space and bicycle space requirements.

**A4.2** Garages designed at the rear of the lot.

**3.7.20.4 Building Design Requirements**

**3.7.20.4.1 Building Height, Form, Scale & Bulk**

**3.7.20.4.1.1 Statement**

*Dwellings that contribute positively to the character of the Richmond Raceway Precinct are generally of similar form, bulk and scale. The prevailing form is modest single and double storey brick residences orientated towards the street. Single and two storey dwellings are compatible in the Richmond Raceway Precinct. Lot sizes and setbacks are also consistent throughout the Precinct. In any new development, the form, bulk and scale will need to be demonstrably compatible with the existing and surrounding residences.*



**3.7.20.4.1.2 Desired Development Outcomes**

- i. New developments should reflect the prevailing form, bulk and scale of the immediate locality; and,
- ii. New developments shall respect and follow the predominant street pattern in terms of roof pitch, orientation and articulation.

### 3.7.20.4.1.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> New developments, additions and alterations to be of a compatible form, bulk and scale to existing development in the immediate locality.</p>	<p><b>A1.1</b> Developments to comply with all design elements of this Local Planning Policy.</p> <p><b>A1.2</b> Additions and alterations are single storey or double story.</p> <p><b>A1.3</b> Category 'B' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable as the 'Acceptable Development' standards.</p> <p><b>A1.4</b> Category 'A' provisions as set out within Table 3 – Maximum Building Heights of the Residential Design Codes are applicable for development on battle-axe lots. Where upper level portions are nonetheless applied for, consideration shall be given to the relaxation of heights in Category 'A' where the following apply:</p> <ul style="list-style-type: none"> <li>i. The proposal demonstrates design, bulk and scale that responds to the established character or other site specific circumstances;</li> <li>ii. The provision of a landscaping plan demonstrating a minimum of 50% of the effective lot area being landscaped; and,</li> <li>iii. Subject to the provisions of Residential Design Codes – Element 9 – Design for Climate and Element 8 – Privacy.</li> </ul>
<p><b>P2</b> Direct overlooking of active habitable spaces and outdoor living areas of other dwellings is minimized by building layout, location and design of major openings and outdoor active habitable spaces, screening devices and landscape, or remoteness.</p>	<p><b>A2</b> Major openings to active habitable spaces or their equivalent which have a floor level more than 0.5m above natural ground level and positioned so as to overlook any part of any other residential property behind its street setback line, to comply with at least one of the following:</p> <ul style="list-style-type: none"> <li>i. are set back, in direct line of sight within the cone of vision, from the boundary of an adjoining property (Refer R Codes Section for setback distances).</li> </ul>

**P3** Development designed with regard for solar access for neighbouring properties taking account the potential to overshadow:

- i. outdoor living areas;
- ii. major openings to habitable rooms;
- iii. solar collectors; and,
- iv. outdoor habitable areas.

Or

- ii. are provided with permanent vertical screening to a height of 1.6m to restrict views from any major opening of an active habitable space.

**A3** Notwithstanding the boundary setbacks developments shall be designed that its shadow cast at midday, 21 June onto any other adjoining property does not exceed the following limits:

- i. on adjoining properties coded R25 and less – 25% of the site area; and,
- ii. on adjoining properties coded R30 to R40 inclusive – 35% of the site area.

### 3.7.20.4.2 Verandahs and Porches

#### 3.7.20.4.2.1 Statement

*Verandahs are a strong contributor to the character of the Richmond Raceway Precinct and should be encouraged in new developments. Most verandahs are corrugated iron, bullnose or skillion roofed and are located on the front façades of residences as independent elements from the main roof.*



#### 3.7.20.4.2.2 Desired Development Outcomes

- i. Verandahs and porches to be encouraged in new developments. They should complement the immediate locality.

### 3.7.20.4.2.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> The principle façade and main entrance of a new building should address the street in a manner that reflects the predominant character of the traditional development in the immediate locality.</p>	<p><b>A1</b> New developments to include a verandah or porch that address the primary street and comprises the primary access to the building.</p>
<p><b>P2</b> Verandah or porch to be a separate element from the existing dwelling.</p>	<p><b>A2</b> Verandah to be constructed below roof line.</p>
<p><b>P3</b> Contributory buildings retain their primary access and conserve the traditional detailing of verandahs and porches.</p>	<p><b>A3</b> Additions and alterations to a contributory building to retain or reinstate the existing verandah or porch. Where appropriate and information available restore/reinstate their original detailing.</p>

### 3.7.20.4.3 Fremantle Port Buffer

#### 3.7.20.4.3.1 Statement

*Fremantle Ports undertook the Fremantle Inner Harbour Definition Study (May 2002), which identified the need for an offsite buffer around the Port. The buffer was determined on a range of potential amenity impacts and risks including noise, odour and public risk.*

*Three buffer areas around the Port have been identified; Area 1, Area 2 and Area 3. For the purposes of these Guidelines these areas will be known as Buffer Area 1, Buffer Area 2 and Buffer Area 3. The buffer area relevant to the Richmond Raceway Precinct is Buffer Area 3 (refer figure 32).*

*Generally the potential risk and amenity impacts from the Port are considerably less in Buffer Area 3. Nevertheless, the Fremantle Inner Harbour Buffer Definition Study has identified the potential for some noise and odour impacts in this area.*

*The intent of Buffer Area 3 is the management, as opposed to the control, of sensitive uses.*



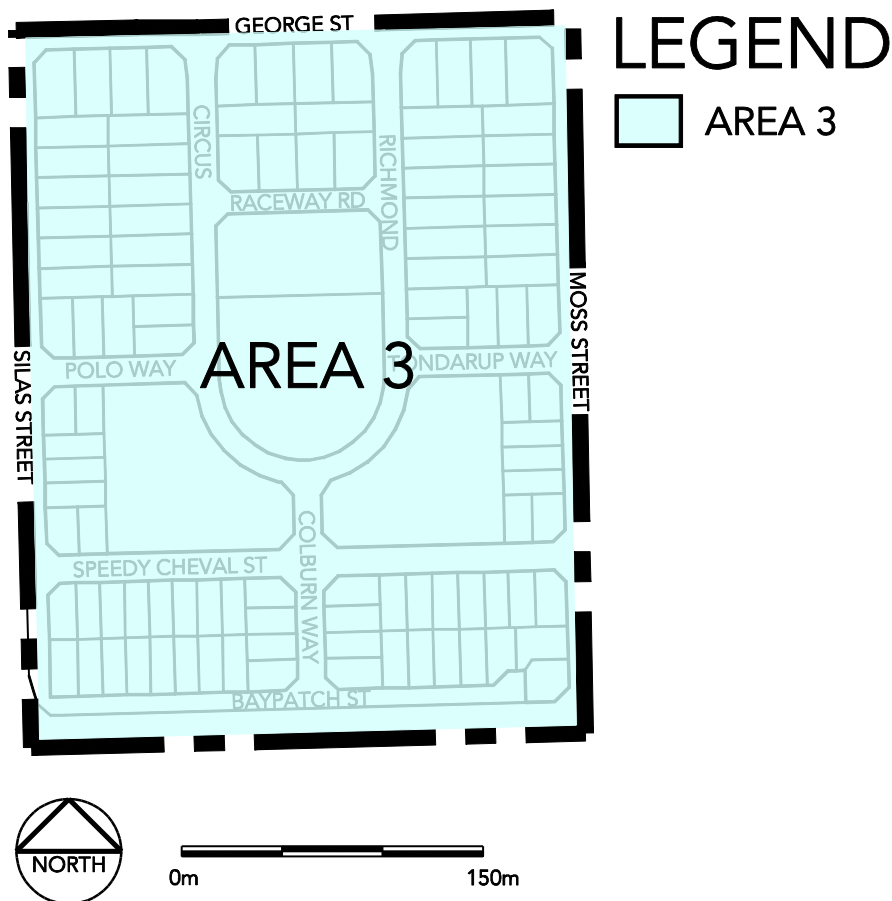


Figure 32- Fremantle Port Buffer Zone Area 3 affecting the Richmond Raceway Precinct. *Griffiths Architects 2011*

### 3.7.20.4.3.2 Desired Development Outcomes

- i. To minimise potential impacts that may arise from the Port;
- ii. To define separate land use and built form requirements for each buffer area; and,
- iii. To manage sensitive uses.

### 3.7.20.4.3.3 Performance Criteria and Acceptable Development Provisions

Performance Criteria	Acceptable Development Provisions
<p><b>P1</b> There are no general buffer related development controls for Buffer Area 3. However, where a specific location within this area is known to be impacted from port operations (e.g. through a history of formal complaints), Council may, in consultation with Fremantle Ports, apply some or all of the development controls of Buffer Area 2.</p>	<p><b>A1.1</b> Developments, additions and alterations must comply with the Fremantle Buffer Area requirements.</p> <p><b>A1.2</b> Council may accept alternative built form treatments subject to the applicant satisfactorily demonstrating fulfilment of the potential risk and amenity considerations outlined above. Alternative treatments shall be justified to Council through submission of professionally prepared and certified reports.</p>

#### 3.7.20.4.3.4 Administrative Procedures

Where applicable, applicants should be advised as soon as possible of the requirements of this policy. Ideally, this should be prior to lodging a formal application for development, including proposals for subdivision and scheme amendments.

Applicants should be encouraged to liaise with relevant staff including those at Fremantle Ports, in order to understand the requirements of this policy.

Council shall refer a proposal to Fremantle Ports where a proponent seeks any significant variation to the development controls contained within these guidelines.

Fremantle Port shall within 14 days of notification, advise the Town of its assessment of a development proposal referred as per the requirements outlined above.

In terms of conditions of development approval that arise from the requirements of this policy, Council shall require a building surveyor or suitably qualified structural engineer to certify that the requirements of the conditions have been fulfilled in accordance with the approved plans.

Where appropriate, certification shall be provided prior to the issue of a building permit, certificate of clearance / classification or strata / subdivision clearance.

The applicant shall arrange for certification to be endorsed by Fremantle Ports prior to lodgement of appropriate documentation with the Town.

#### Buffer Area 3

Council shall refer a proposal to Fremantle Ports where the proposal falls within a specific location that has been formally notified to Council as being impacted from port operations.

In the case of scheme amendments that would result in an increase or intensification of sensitive uses, Council shall notify Fremantle Ports as soon as practicable prior to initiating the amendment.



### **3.7.21 Development Applications**

#### **3.7.21.1 Requirements**

The definition of 'development' is set out in the Planning and Development act 2005. The requirements for submissions of Development Applications, are as set out in TPS3.

All developments require a development application and approval unless exempted from the need for an application under the Scheme.

#### **3.7.21.2 Accompanying Material**

The Town may require an applicant to provide one or more of the following to assist the Town in the determination of a planning application. This is additional to the requirement for accompanying material set out in Clause 9.2 of TPS 3.

#### **3.7.21.3 Heritage Assessment**

A Heritage Assessment may be requested at the applicant's expense for places on the Heritage Schedule adopted under TPS 3.

#### **3.7.21.4 Heritage Impact Statement**

A Heritage Impact Statement to be prepared at the applicant's expense may be requested for places on the Schedule attached to TPS 3 where development is likely to have a substantial impact on the cultural heritage values of that place.

#### **3.7.21.5 Conservation Plan**

Where a Conservation Plan has been prepared for individual buildings, groups of buildings or places of cultural heritage significance (refer glossary), the Town will take the content of the Conservation Plan into account when determining development applications for the places to which they apply. Where Conservation Plans are available, these, or relevant sections of these, should be provided with the development application.

The Town may require the preparation of a Conservation Plan prior to considering a development application for all buildings or places that are included on the Heritage Council's Register of Heritage Places.

#### **3.7.21.6 Structural Condition Assessment in the Case of Demolition**

If structural failure is cited as a justification for the demolition of a place on the Schedule adopted under TPS3, evidence shall be provided from a registered structural engineer with experience in dealing with heritage places, that the structural integrity of the building has failed, to the point where it cannot be rectified without removal of a majority of its significant fabric and/or incurring prohibitive costs. Structural engineers should be selected from the Heritage Council of Western Australia's preferred list of consultants.

This information is additional to the requirements for accompanying material for a development application set out in Clause 9.2 of TPS3.

#### **3.7.21.7 Viewshed Study**

*Where a development has the capacity to impact on viewsheds, Council may request the inclusion of a viewshed study as part of the development requirements.*

*Precincts where this is most likely to be a requirement include Riverside, Preston Point and Richmond Hill north abutting Preston Point Road.*

### **3.8 Policy Date Review**

Three years from adoption date.

### 3.9 APPENDIX A – Development Impact Statement

#### Introduction

Brief description of proposed development.

#### Reasons for Demolition

#### Heritage Listings

Refer to Town of East Fremantle's Municipal Heritage (MHI) Inventory for listings.

#### Statement of Significance

Refer to Town of East Fremantle's MHI for a Statement of Significance.

#### Description of the Place

Describe:

- Street and the context in which the places were built. Describe scale and construction of dwellings.
- Immediate historic and visual relationship of dwellings in the street.
- Significant garden features.
- Condition of existing house in terms of wall and roof construction and any important features of the house i.e. verandahs, gables and roof forms, and
- Any previous additions.

#### Heritage Values

Heritage values	
Aesthetic	Degree of significance located on the MHI.
Historic	Degree of significance located on the MHI.
Social	Degree of significance located on the MHI.
Scientific	Degree of significance located on the MHI.
Heritage Attributes	
Rarity	Degree of significance located on the MHI.
Representativeness	Degree of significance located on the MHI.
Integrity	Degree of significance located on the MHI.

Authenticity	Degree of significance located on the MHI.
--------------	--------------------------------------------

Significance			
Exceptional	Considerable	Some	Limited or none
Comments:  Tick one of the above and provide comment.			

### Statement of Heritage Impact

How does the proposed development impact on the heritage significance of the place with regard to the following criteria:	
Degree of change (positive and negative) on the place in light of its heritage significance.	
Degree of permanent impact (irreversible loss of value) that the proposal is likely to have on the heritage significance of the place.	
Compatibility with heritage building in terms of scale, bulk, height – the degree to which the proposal dominates, is integrated with, or is subservient to a heritage place.	
Compatibility with the streetscape and/or heritage area in terms of the siting, local architectural patterns, and the degree of harmonised integration of old and new. Compatibility with viewsheds.	
Compatibility with heritage building in terms of the design solutions and architectural language such as refinement and finesse of detailing, texture, materials, finishes and quality of craftsmanship.	
Degree of impact on the important public views, vistas, landmarks, landscape features.	

INSERT PICTURES OF EXISTING BUILDING

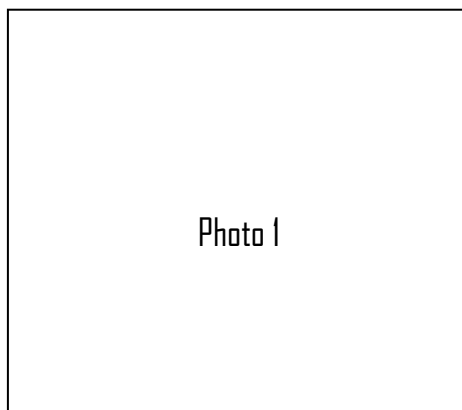


Photo 1 – Street façade photo 1

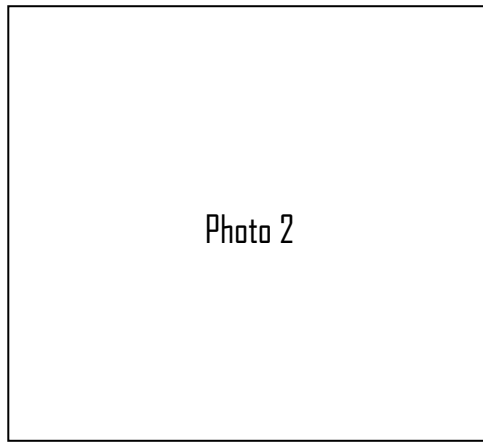


Photo 2 – Street façade photo 2

### 3.10 GLOSSARY

Unless the context otherwise requires, words and expressions used in this Local Planning Policy have the same meaning as they have:

- Planning and Development Act 2005,
- Residential Design Codes,
- Building Code of Australia,
- Relevant Australian Standard, or
- Heritage of Western Australia Act 1990.

The meaning of other specific words and expressions relevant to this Local Planning Policy are given below:

#### **Abutment**

An intersection between a roof slope and a wall that rises above it.

#### **Archival Record**

A document containing drawings, photographs and written information prepared in order to record the state of a place at a given time, usually prior to demolition or major change.

#### **Building**

For the purposes of this Local Planning Policy the term “building” shall have the same meaning as given in the Heritage of Western Australia Act 1990:

“in relation to any land, includes any structure erected or placed on or in that land, and any part of the building or fence or other appurtenance to the building.”

#### **Building Envelope**

A building envelope is the separation between the interior and the exterior environments of a building or its outer shape.

#### **Burra Charter**

The Australian chapter of the International Council of Monuments and Sites (ICOMOS) Charter for the conservation of places of cultural significance. The Charter has been generally accepted as the standard for heritage practitioners in Australia.

#### **Conservation**

Under the Heritage of Western Australia Act 1990 the term “Conservation” means all the processes of looking after a place so as to retain its cultural significance. It includes maintenance and may, according to circumstances, include preservation, restoration, reconstruction and adaptation. Conservation will commonly involve a combination of more than one of these.

### **Conservation Plan**

A document that details how to identify and look after the significant cultural values of a place. Its preparation involves a systematic way of considering, recording and monitoring actions and decisions relating to all aspects of managing a place. The Heritage Council of WA provides guidelines for the preparation of Conservation Plans to ensure that all important matters are considered.

### **Conservation Plan**

The aesthetic, historic, social and scientific values of a place for past, present or future generations (Heritage of Western Australia Act 1990).

### **Contributory Building**

A building that appears on the Town of East Fremantle's Municipal Heritage Inventory.

### **Contributory Fence**

An original fence to a contributory building.

### **Cultural Heritage Significance**

Cultural Heritage Significance means, in relation to a place, the relative value which that place has in terms of its aesthetic, historic, scientific, or social significance, for the present community and future generations

### **Cultural Place**

A site, area, building or other work valued for its cultural heritage significance, together with associated contents and surrounds.

### **Demolition**

Demolition means the disassembling or removal in whole or part of any building from a site or part of a building from the remainder of the building.

### **Development**

Development means the development or use of any land, including any demolition, erection, construction, alteration of or addition to any building or structure on the land and the carrying out on the land of any excavation or other works and, in the case of a place to which a Conservation Order made under s59 of the Heritage of Western Australia Act 1990 applies, also includes any act or thing that:

- Is likely to change character of that place or the external appearance of any building, or
- Would constitute an irreversible alteration of the fabric of any building.

### **Development Impact Statement (Heritage & Character)**

Also referred to as a Heritage Impact Statement. It is a report which has been undertaken by an appropriately qualified person or body as part of any application for planning approval that evaluates the likely impact of proposed development on the significance of a heritage place and its setting, or on the Heritage Area within which it is situated. The report may also outline measures by which any detrimental impact may be minimised.

### **Experienced Heritage Professional**

A person or organisation that is included in the Heritage Council of Western Australia's approved list.

### **Faux**

Not genuine or real; being an imitation of the genuine article.

### **Faux Heritage**

Faux heritage buildings imitate the physical features of existing buildings. However, they detract from and devalue the significance of authentic heritage buildings.

### **Gable**

The triangular part of the end wall of a building with a pitched roof between the barge boards or rafters. A gable may be of any material.

### **Gable Roof**

A roof with gables at one or both ends. The edge of the roof is covering the verge.

### **Gablet**

A small gable.

### **Gambrel Roof**

An end to a pitched roof which slopes up from the eaves, as does a hipped end, but stops part way at a vertical gablet. The shape resembles a horse's hind leg.

### **Heritage**

The evidence of the past, such as historical sites, buildings, and the natural environment, considered collectively as the inheritance of present-day society.

### **Heritage Agreement**

A contract under section 29 of the Heritage of Western Australia Act 1990 (the "Act") which is undertaken on a voluntary basis by the owners of a heritage place. The agreement binds current and successive owners to a set of conservation conditions and may provide compensating benefits in some circumstances. Essentially, the purpose of a Heritage Agreement is to secure the long-term conservation of a heritage place. A Heritage Agreement runs with the land and is confirmed through a Memorial placed on the Certificate of Title.

### **Heritage Assessment**

A systematic assessment that describes a place and its setting and states its significant heritage values (level of contribution) in terms of the criteria adopted by the Heritage Council of Western Australia. These criteria are the aesthetic, historic, social and scientific values of the place.



## **Heritage Council of Western Australia**

The State Government's advisory body on heritage matters and is responsible for the State Register of Heritage Places. Under the Heritage of Western Australia Act 1990, places listed in the State Register are given legal protection. The Heritage Council provides advice to the Minister for Heritage on heritage issues identifies and conserves places of cultural heritage significance, facilitates development that is in harmony with cultural heritage values, and promotes awareness and knowledge of cultural heritage.

## **Heritage of Western Australia Act 1990 (the "Act")**

The statutory framework for the identification and conservation of places which have significance to the cultural heritage of Western Australia. The Act also describes the composition and powers of the Heritage Council of WA and requires Local Governments to prepare Municipal Heritage Inventories.

## **Heritage Place**

A building, structure, site, area of land or other physical element valued for its cultural (or historic) heritage significance, together with associated contents and surrounds.

## **Hipped Roof**

A pitched roof which has four slopes instead of two slopes of an ordinary gabled roof. The shorter sides are roofed with sloping triangles, the hipped ends, each bounded by two hips from eaves to ridge, and by eaves below.

## **Interpretation**

All the ways of presenting the significance of a heritage place. Interpretation can include the use of colour, lighting, furnishings, historic material or signage or a combination of these to tell the story of the building or place.

## **Interpretation Plan**

A document that explains the ways in which a place could be interpreted. Interpretation Plans should aim to increase both our understanding and our enjoyment of heritage places.

## **Lean-to roof, half-span**

A pitched roof sloping one way only, with its top edge meeting a wall higher than the roof, forming an abutment.

## **Local Government**

The Town of East Fremantle, or the Council of Town of East Fremantle, as the context requires.

## **Municipal Heritage Inventory (MHI)**

A list of places within the Town of East Fremantle, considered by the Town to be of identified cultural heritage significance and worthy of conservation.

## **National Trust of Australia (WA)**

An independent community organisation established under the National Trust of Australia (WA) Act 1964, and is part of a world-wide movement of National Trusts. The Trust identifies and maintains a list of premises of heritage significance to the State, and is custodian of heritage assets vested in it. The Trust also has a traditional educational role in promoting community concern for, and understanding of, the State's heritage.

The Trust does not have the power to impose preventative or restrictive conditions on listed places, however it can lobby on behalf of the community for governments and interested parties to act towards this effect.

## **Natural Ground Level**

At any point for the purposes of determining the height of a wall or building, means the ground level which existed prior to development (including any earth works), and shall be interpolated based on the natural ground level is to be determined. Where there is a level of difference at or immediately adjacent to the boundary, the natural ground level at the boundary shall be taken to be ground level on the lower side of the boundary, unless it can be demonstrated to the satisfaction of the local government, that the lower level has resulted from excavation undertaken after the subdivision by which the relevant boundary was established.

## **Pergola**

An unroofed open frame structure.

## **Pitched Roof**

The commonest roof usually one with two slopes at more than 20° to the horizontal, meeting at a central ridge. It may have gables or hips.

## **Plinth**

A slab or block that forms the lowest part/base of a structure.

## **Plot Ratio**

Defined by Statement of Planning Policy No. 3.1: Residential Design Codes, means the ratio of the gross total of the areas of all floors of buildings on a site to the area of land within the site boundaries. For this purpose, such areas shall include the area of any walls but not include the areas of lift shafts, stairs or stair landings common to two or more dwellings, machinery, air conditioning and equipment rooms, non-habitable space that is wholly below natural ground level, areas used exclusively for the parking of wheeled vehicles at or below natural ground level, lobbies or amenities areas common to more than one dwelling, or balconies or verandahs open on at least two sides.

## **State Planning Policy 3.5 – Historic Heritage Conservation**

The objectives of the Policy are:

- To conserve places and areas of historic heritage significance,
- To ensure that development does not adversely affect the significance of heritage places and areas,
- To ensure that heritage significance at both the state and local levels is given due weight in planning decision-making, and
- To provide improved certainty to landowners and the community about the planning processes for heritage identification, conservation and protection.

## Policy Area

The Policy Area is the zone defined by Marmion Street, Petra Street, Jerratt Drive, Riverside Road and East Street.

## Storey

Defined by Statement of Planning Policy No. 3.1: Residential Design Codes, means that part of a building between floor levels. If there is no floor above, it is the part between the floor level and the ceiling.

A “storey” when used in relation to a development that is for residential purposes has the same meaning as Statement of Planning Policy No. 3.1: Residential Design Codes, meaning the vertical distance at any point from natural ground level to the uppermost part of the building above that point (roof ridge, parapet or wall), excluding minor projections above that point.

## Structural Condition Assessment

A report prepared by a qualified structural engineer that assesses the structural state of a building or element.

## Immediate Locality

The five premises on either side of the proposed development on both sides of the street that the subject building is oriented towards. More significance will be given to development in the immediate locality that is closer to the proposed development, particularly in the case of corner lots.

## Traditional

Traditional means the predominant historical development type in areas where there is precinctual heritage value.

## Vertical Profile

The height is greater than the width.

## Visually Permeable

Continuous vertical gaps of at least 50mm width occupying not less than 60% of the face in aggregate of the entire surface. That is at least 60% of the wall must be open.

*NOTE: This differs from the R-Codes.*

## Viewshed

A **viewshed** is an area of land, water, or other **environmental** element that is visible to the human eye from a fixed vantage point. In this case, viewsheds are areas of particular scenic or historic value that are deemed worthy of preservation against development or other change.

Viewsheds are spaces that are readily visible from public areas such as from public roadways, public parks or say the north side of the Swan River.

## **Zones of Significance**

Zones of significance identify the level of significance of spaces and elements in existing buildings. The elements are considered within their historical and physical context and in relation to the significance of the place as a whole.